



Planning & Development Services

Boise City Hall, 2nd Floor
150 N. Capitol Boulevard
P. O. Box 500
Boise, Idaho 83701-0500

Phone: 208/384-3830
Fax: 208/384-3753
TDD/TTY: 800/377-3529
Website: www.cityofboise.org/pds

8/8a

CAR16-00030 & PUD16-00027 / Vista Village, LLC

Summary

The applicant is requesting to rezone the subject property from an R-1C (Single Family Residential) to an R-2D (Medium Density Residential with Design Review) zone. In addition, the applicant requests a conditional use permit for a 91-unit planned residential development on ± 7.39 acres located at 2350 W. Kootenai Street.

Prepared By

Leon Letson, Associate Planner

Recommendation

The Planning Team recommends **approval** of each request.

Reason for the Decision

Rezone

The rezone is consistent with the Comprehensive Plan. The property is designated “Compact” on the Land Use Map. R-2D is a permissible implementing zone in this designation. The rezone is also in the best interest of the public. The property is currently zoned R-1C (Single Family Residential), which has a maximum residential density of eight units per acre. The change in zoning will accommodate new residents that can make use of existing services, infrastructure, and amenities in the area. Finally, the rezone will maintain and preserve the compatibility of surrounding zoning and development. The subject property is adjacent to a number of more intense zones, including R-3D (Multi-Family Residential with Design Review) to the north, L-OD (Limited Office with Design Review) to the east, and C-2D (General Commercial with Design Review) to the west.

Conditional Use Permit

This multi-family development is compatible with the uses in the neighborhood and those reasonably expected to develop. To the west is the Vista Village Shopping Center and the commercial corridor of Vista Avenue. The remainder of the surrounding neighborhood is comprised of a mix of single-family and smaller multi-family uses. There are also a number of vacant parcels with more intense zoning that could support smaller infill projects. The project is also consistent with the *Depot Bench Neighborhood Plan* and *Blueprint Boise*. The *Depot Bench Neighborhood Plan* identifies the subject property as a target area for medium-density housing projects. Multiple principles within *Blueprint Boise* encourage a mix of housing in neighborhoods and infill development that does not require the costly extension of infrastructure. All necessary utilities and infrastructure are readily available to the site. The location of this project adjacent to existing railroad right-of-way also aligns with *Principle CC5.1(c)* which seeks to establish a future multi-modal transportation system that includes light rail in the Treasure Valley.

In line with the requirements of the *Citywide Design Standards and Guidelines*, parking will be located internal to the site with appropriate screening of service drives. The buildings themselves include modulation in façade and rooflines to reduce their perceived mass, as well as appropriate

materials, openings and covered entries, consistent with the surrounding neighborhood. Both the *Depot Bench Neighborhood Plan* and *Principle IDP-N.1(a)* of *Blueprint Boise* encourages transitions in building heights and the use of variations in side yard setbacks to ensure infill development in established neighborhoods is compatible. The applicant has proposed a design that focuses taller, 3-story buildings at the center of the site, stepping down to 2-story buildings along the edges of the site adjacent to areas with single-family homes. All buildings within the development will be setback more than 70' from adjacent properties. Finally, comments from public agencies confirm the project will not place an undue burden on the transportation system or other infrastructure in the neighborhood.

This report includes information available on the Boise City Website. The entire public record, including additional documents, can be viewed through PDS Online through the following link:

<http://pdsonline.cityofboise.org/pdsonline/Permits.aspx?id=0>

R-1C

R-2

8/8a

R-OD

C-1D

C-1D

R-3D

L-OD

R-1C

A-1

R-3D

CAR16-00030 & PUD16-00027
1" : 300'



W Gem St

W Cassia St

W Lampert Dr

W Wymer St

W Camas St

W Edson St

W Edson St

W Kootenai St

W Teton St

S Victoria Dr

W Willoway Dr

S Mesa Vista Dr

S Mesa Vista Dr

Ridenbaugh Canal

Federal Way Trail
S Federal Way

S Protest Rd

Federal Way ROW

Terry Day Park

S Robert St

S Wade Cir

S Day Dr

W Day Dr

S Shaw Dr

S Manor Dr

W Kootenai St

W Pendleton St

S Helen St

W Jean St

S Leta St

S Columbus St

S Vista Ave

S Broxon St

S Opal St

S Abbs St

S Broxon St

R-1C

W Gem St

S Victoria Dr

W Willoway Dr

W Boise Ave

U

R-2

8/8a

R-OD

C-1D

C-1D

CAR16-00030 & PUD16-00027
1" : 300'



W Cassia St

W Lampert Dr

W Wymer St

C-2D

S Robert St

S Wade Cir

S Day Dr

S Mesa Vista Dr

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Ridenbaugh Canal

S Protest Rd

R-3D

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S Vista Ave

W Day Dr

R-1C

Federal Way Trail
S Federal Way

S Pico St

L-OD

W Edson St

W Edson St

S Broxon St

S Shaw Dr

S Manor Dr

Federal Way ROW

W Kootenai St

W Kootenai St

R-1C

R-3D

A-1

Terry Day Park

R-3D

S Abbs St

S Broxon St

W Teton St

W Pendleton St

S Helen St

W Jean St

S Leta St

S Columbus St

#117: Planned Unit Development

Case #: PUD16-00027

8/8a

Property Information

Address

Street Number: 2350	Prefix: W	Street Name: KOOTENAI ST	Unit #: 			
Subdivision name: SEC 15 3N 2E	Block: 	Lot: 	Section: 15	Township: 3	Range: 2	Zoning: R-1C
Parcel Number: S1015325410	Additional Parcel Numbers: S1015325550					

Primary Contact

Who is responsible for receiving e-mail, uploading files and communicating with Boise City?

☒ Agent/Representative ☐ Applicant ☐ Owner

Applicant Information

First Name: Donna	Last Name: Jacobs		
Company: Vista Village, LLC.			
Address: P.O. Box 8286	City: Boise	State: ID	Zip: 83707
E-mail: donnadjacobs@gmail.com	Phone Number: (208) 342-3528	Cell: 	Fax:

Agent/Representative Information

Role Type: ☒ Architect ☐ Land Developer ☐ Engineer ☐ Contractor ☐ Other

First Name: Robert	Last Name: Powell		
Company: Dave Evans Construction			
Address: 7761 W. Riverside Dr. #100	City: Boise	State: ID	Zip: 83714
E-mail: scottg@devansconstruction.com	Phone Number: (208) 853-1203	Cell: (208) 870-4161	Fax: (208) 853-1220

Owner Information

Same as Applicant? ☐ No ☒ Yes (If yes, leave this section blank)

First Name: 	Last Name: 		
Company: 			
Address: 	City: 	State: ID	Zip:
E-mail: 	Phone Number: 	Cell: 	Fax:

Is this a Modification application?

☐ Yes☒ No

File number being modified:

8/8a

1. Neighborhood Meeting Held (Date):

7/19/16

2. Neighborhood Association:

Depot Bench

3. Comprehensive Planning Area:

Central Bench

4. This application is a request to construct, add or change the use of the property as follows:

Construct a 91 Unit multi-family development

5. Size of Property:7.39 ☒ Acres ☐ Square Feet**6. Water Issues:**

A. What are you fire flow requirements? (See International Fire Code):

1500 gpm

B. Number of hydrants (show location on site plan):

Note: Any new hydrants/hydrant piping require United Water approval.

Number of Existing: 0 Number of Proposed: 3

C. Is the building "sprinklered"?

☒ Yes☐ No

D. What volume of water is available? (Contact United Water of Idaho at 362-7330):

1500 gpm

7. Existing uses and structures on the property are as follows:

Vacant land, railroad track spurs

8. Are there any hazards on the property?

(Such as canals, hazardous material spills, soil or water contamination.) If so, describe them and give their locations:

n/a

9. Adjacent property information:

	Building types and/or uses	Number of Stories	Zone
North:	Residential Single	1	(R-1C) Single Family Resider
South:	W. Kootenai St /	2	(R-1C) Single Family Resider
East:	Railroad Track R	1	(A) Open Land
West:	Retail Commercial	1	(C-2D) General Commercial

A. Number of **Proposed** non-residential structures: 1

Square footage of proposed non-residential structures or additions (if 5+ floors, attach narrative with chart):

	Gross Square Feet	Net Leasable Square Feet
1st Floor	1620	0
2nd Floor	0	0
3rd Floor	0	0
4th Floor	0	0

B. Maximum Proposed Height: 25

C. Number of stories: 2

D. Number of **EXISTING** non-residential structures to remain: 0

Square footage of existing non-residential structures or additions (If 5+ floors, attach narrative with chart):

	Gross Square Feet	Net Leasable Square Feet
1st Floor	0	0
2nd Floor	0	0
3rd Floor	0	0
4th Floor	0	0

E. Existing Structure Height(s): 0

F. Number of Stories: 0

11. Residential Structures:A. Number of **Proposed** residential units (if applicable):: 5

B. Size of Proposed residential structures (if applicable):

	Number of Units	Square Foot per Unit	Total Square Feet
One-Bedroom:	36	758	27288
Two-Bedroom:	39	1009	39351
Three-Bedroom:	16	1230	19680
Other:	1	00	00
Total Number:	0	0	0

C. Number of **Existing** units to remain: 0

D. Maximum Proposed Structure Height(s): 39

E. Number of Stories: 3

A. Percentage of site devoted to building coverage:	<input type="text" value="11.2"/>
B. Percentage of site devoted to landscaping:	<input type="text" value="43.4"/>
C. Percentage of site devoted to paving:	<input type="text" value="22"/>
D. Percentage of site devoted to other uses:	<input type="text" value="23.4"/>
E. Describe other use:	<input type="text" value="Sidewalks, basketball court & railroad tracks"/>

13. Loading Facilities, if proposed (For Commercial uses only):

Number:	<input type="text"/>	Location	<input type="text"/>
Size:	<input type="text"/>	Screening	<input type="text"/>

14. Parking:

	Required		Proposed
A. Handicapped Spaces:	<input type="text" value="3"/>	Handicapped Spaces:	<input type="text" value="5"/>
B. Parking Spaces:	<input type="text" value="119"/>	Parking Spaces:	<input type="text" value="144"/>
C. Bicycle Spaces:	<input type="text" value="91"/>	Bicycle Spaces:	<input type="text" value="93"/>
D. Proposed Compact Spaces:	<input type="text" value="48"/>		
E. Restricted (assigned, garage, reserved spaces) parking spaces proposed:			<input type="text" value="91"/>
F. Are you proposing off-site parking?	<input type="radio"/> Yes <input checked="" type="radio"/> No	If yes, how many spaces?	<input type="text"/>
G. Are you requesting shared parking or a parking reduction?	<input type="radio"/> Yes <input checked="" type="radio"/> No	If yes, how many spaces?	<input type="text"/>

Note: If you are requesting shared parking or a parking reduction, you must submit a survey of persons using and working on the premises and any additional information demonstrating that use by the regular employees and visitors to the premises will require fewer off-street parking spaces than required by the Zoning Ordinance.

15. Setbacks (Plans that are not graphically dimensioned will not be accepted.)

	Building		Parking	
	Proposed	Required	Proposed	Required
Front:	<input type="text" value="200"/>	<input type="text" value="20"/>	<input type="text" value="100"/>	<input type="text" value="10"/>
Rear:	<input type="text" value="20"/>	<input type="text" value="15"/>	<input type="text" value="20"/>	<input type="text" value="10"/>
Side 1:	<input type="text" value="76"/>	<input type="text" value="15"/>	<input type="text" value="70"/>	<input type="text" value="10"/>
Side 2:	<input type="text" value="70"/>	<input type="text" value="15"/>	<input type="text" value="62"/>	<input type="text" value="10"/>


16. Waivers Requested:

A. Lot size:	<input type="radio"/> Yes <input checked="" type="radio"/> No	Description:	<input type="text"/>
B. Internal Setbacks:	<input type="radio"/> Yes <input checked="" type="radio"/> No	Description:	<input type="text"/>
C. Frontage:	<input type="radio"/> Yes <input checked="" type="radio"/> No	Description:	<input type="text"/>

17. Sidewalks:

Proposed: ☒ Attached ☐ Detached
 Adjacent: ☒ Attached ☐ Detached

18. Amenities:

Number:
 Description: 

19. Density:

Allowed Density:
 Proposed Density:

20. Building Exterior:

	Materials	Colors
Roof:	<input type="text" value="Asphalt shingle"/>	<input type="text"/>
Walls:	<input type="text" value="Hardi-Siding"/>	<input type="text"/>
Windows/Doors:	<input type="text" value="Vinyl / Fiberglass"/>	<input type="text"/>
Fascia, Trim etc.:	<input type="text" value="Hardi-Board"/>	<input type="text"/>

21. Drainage (Proposed method of on-site retention):
22. Floodways & Hillside:

- A. Is any portion of this property located in a Floodway or a 100-year Floodplain? ☐ Yes ☒ No
- B. Does any portion of this parcel have slopes in excess of 15%? ☐ Yes ☒ No

Note: If the answer to either of the above is yes, you will be required to submit an additional Floodplain and/or Hillside application and additional fee. You must submit the additional required application(s) for review at the same time as this request.

23. Airport Influence Area:

Is the subject site located within the Airport Influence Area? (If yes, please mark which area.)

☒ No ☐ Area A ☐ Area B ☐ Area B1 ☐ Area C

A. PUBLIC Street Layout Review

The impacts of proposed development on adjacent land uses and transportation facilities must be considered. A "Traffic Impact Study" (TIS) will be generally required by the Ada County Highway District, if the proposed development contains no more than 1,000 dwelling units (includes hotels and motels as well as private dwelling units), more than 30,000 square feet of commercial use, or more than 50,000 square feet of industrial or institutional use, or has associated it with special circumstances deemed by ACHD to warrant an impact study. A copy of this study must be submitted with this application.

Is a Traffic Impact Study required?

☐ Yes ☒ No

B. PRIVATE Street Layout Review

The impacts of proposed development on adjacent land uses and transportation facilities must be considered. A "Traffic Impact Study" (TIS) prepared by a traffic engineer will be required by Public Works and Planning & Development Services for the interior roadway and parking system. This requirement may be waived when it can be shown by the applicant that no section of on-site roadway will exceed 240 vehicle trips per day.

Is a Traffic Impact Study required?

☐ Yes ☒ No

Are you proposing public street connection to adjacent properties?

☐ Yes ☒ No

25. Solid Waste:

A. Type of trash receptacles:

☐ Individual Can/Residential ☐ 3 Yd Dumpster ☐ 6 Yd Dumpster ☒ 8 Yd Dumpster ☐ Compactor

B. Number of trash receptacles:

4

C. Proposed screening method:

CMU wall/ landscaping

D. Is the proposed location accessible for collection? (Contact Boise Public Works at 384-3901.)

☒ Yes ☐ No

E. Is recycling proposed?

☒ Yes ☐ No

Verification of Legal Lot or Parcel Status

Acceptance of this application does not validate the legal status of any lot or parcel. Prior to submitting for a Building Permit you must have a Verification of Legal Parcel Status form signed by the Boise City Subdivision Department. It is the applicant's responsibility to provide deeds and/or other documentation to the Subdivision Department. See Verification of Legal Lot or Parcel Worksheet for submittal requirements.

The undersigned declares that the above provided information is true and accurate.

The undersigned acknowledges that failure to provide true and accurate information may result in rejection of this application, possible revocation of the permit where wrongfully issued and subject the undersigned any applicable civil and/or criminal penalties.

Agent/Representative Signature:

Date:

Annexation & Rezone Application Form

PDS	Department Application
	# 105 8/8a

New! Type data directly into our forms.

Case #: CAR16-00030

Note: Be sure to print this form before closing it or you will lose your data. This form cannot be saved to your computer.

Property Information

Address: Street Number: 2350 Prefix: W. Street Name: KOOTENAI
Subdivision: 3N 2E 15 Block: Lot: Section: 15 Township: 3N Range: 2E
***Primary Parcel Number:** 51015325410 Additional Parcels: 51015325550

Applicant Information

1110 S. ROBERT STREET
***First Name:** DORNA ***Last Name:** JACOBS
Company: VISTA VILLAGE, LLC. ***Phone:** 342-3528
***Address:** P.O. BOX 8286 ***City:** BOISE ***State:** ID ***Zip:** 83707
E-mail: donnad.jacobs@gmail.com **Cell:** **Fax:**

Agent/Representative Information

First Name: ROBERT **Last Name:** POWELL
Company: DAVE EVANS CONSTRUCTION **Phone:** 853-1203
Address: 7761 W. RIVERSIDE DR. #100 **City:** BOISE **State:** ID **Zip:** 83714
E-mail: Robertp@devansconstruction.com **Cell:** 830-6576 **Fax:** 853-1220
Role Type: ☒ Architect ☐ Land Developer ☐ Engineer ☒ Contractor ☐ Other

Owner Information

Same as Applicant? ☒ Yes ☐ No (If yes, leave this section blank)

First Name: **Last Name:**
Company: **Phone:**
Address: **City:** **State:** **Zip:**
E-mail: **Cell:** **Fax:**

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Date Received:
Revised 10/2008



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CAR 16-00030

8/8a

1. Neighborhood Meeting Held (Date): 7/19/16

2. Neighborhood Association: DEPOT BENCH

3. Comprehensive Planning Area: CENTRAL BENCH

4. This application is a request to construct, add or change the use of the property as follows:

CONSTRUCT A 91 UNIT MULTI-FAMILY DEVELOPMENT -
THE DENSITY FOR THIS REQUIRES A DIFFERENT ZONING DESIGNATION.

5. Type of Request: ☒ Rezone ☐ Annexation & Rezone

6. Current Zone: R-1C

7. Requested Zone: R-2D

8. Size of property: 7.39 ☒ Acres ☐ Square Feet

9. Existing uses and structures on the property are as follows:

VACANT LAND & RAILROAD TRACK SPURS

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10. Are there any existing land uses in the general area similar to the proposed use?

If so, describe them and give their locations:

YES, MULTI-FAMILY OPPOSITE KOOTENAI STREET &
COLOMBUS INTERSECTION. ALSO, DEPOT LOFT APARTMENTS
ON VISTA & VICTORIA DRIVE.

11. On what street(s) does the property have frontage? S. ROBERT ST. ?
CAR 16-00030 KOOTENAI STREET

12. Adjacent Property Information

Uses:

Zone:

North: RESIDENTIAL SINGLE FAMILY

R-1C

South: W. KOOTENAI ST / RAILROADS

R-1C

East: RAIL ROAD TRACKS

A

West: RETAIL COMMERCIAL

C-2D

13. Why are you requesting annexation into the City of Boise?

N/A

14. What use, building or structure is intended for the property?

MULTI-FAMILY DEVELOPMENT, (5) APARTMENT
BUILDING - 2 & 3 STORIES, (1) CLUBHOUSE / MANAGEMENT
OFFICE.

15. What changes have occurred in the area that justify the requested rezone?

DEMAND FOR MULTI-FAMILY HOUSING, HIGHER TRANSPORT-
ATION COSTS, MORE LOCAL SERVICES AVAILABLE IN AREA.

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16. What Comprehensive Plan policies support your request?

IT ALIGNS W/ CB-CCN 1.5: VIST AVENUE CORRIDOR (a) -
HIGHER DENSITY RESIDENTIAL USES, (b) PROVIDES SUPPORT
TO THE EXISTING REHABILITATED VISM VILLAGES & ENCOURAGES
FUTURE REHAB OF OTHERS.

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Applicant/Representative Signature

9/27/16

Date

Print Form

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Planning & Development Services

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Phone: 208/384-3830
Fax: 208/384-3753
TDD/TTY: 800/377-3529
Website: www.cityofboise.org/pds

Affidavit of Legal Interest

State of Idaho)

) ss

County of Ada)

I, USADIVA L.L.C.
Name

P.O. Box 8286
Address

Boise
City

Idaho 83707
State

being first duly sworn upon oath, depose and say:

(If Applicant is also Owner of Record, skip to B)

A. That I am the record owner of the property described on the attached, and I grant my permission to DAVE EVANS CONSTRUCTION 7761 W. RIVERSIDE DR. #100 BOISE
Name Address
to submit the accompanying application pertaining to that property. ID, 83714

B. I agree to indemnify, defend and hold Boise City and its employees harmless from any claim or liability resulting from any dispute as to the statements contained herein or as to the ownership of the property which is the subject of the application.

Dated this 21 day of September, 2016

[Signature]
Signature member VLLC

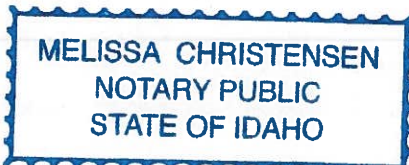
Subscribed and sworn to before me the day and year first above written.

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SEP 27 2016

Melissa Christensen
Notary Public for Idaho

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Residing at: 5/20/20
My commission expires: Nampa ID

CAR 16-00030



Planning & Development Services

Boise City Hall, 2nd Floor
150 N. Capitol Boulevard
P. O. Box 500
Boise, Idaho 83701-0500

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Website: www.cityofboise.org/pds

8/8a

Planning Division Project Report

File Number	CAR16-00030 & PUD16-00027
Applicant	Vista Village, LLC
Property Address	2350 W. Kootenai Street
Public Hearing Date	December 5, 2016
Heard by	Planning and Zoning Commission
Analyst	Leon Letson
Checked By	Cody Riddle

Public Notification

Neighborhood meeting conducted: July 19, 2016
Newspaper notification published on: October 22, 2016
Radius notices mailed to properties within 300 feet on: October 21, 2016
Staff posted notice on site on: October 21, 2016

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Exhibits

Agency Comments
Public Comments

1. Project Data and Facts

Project Data	
Applicant/Status	Vista Village, LLC/Owner
Architect/Representative	Rob Powell/Architect
Location of Property	2350 W. Kootenai Street
Size of Property	±7.39 Acres
Zoning (Proposed)	R-2D (Medium Density Residential with Design Review)
Comprehensive Plan Designation	Compact
Planning Area	Central Bench
Neighborhood Association	Depot Bench/Jim Pickett @ 867-2271
Procedure	The Planning and Zoning Commission renders a final decision on the conditional use permit and makes a recommendation to City Council on the rezone.

Current Land Use
The property is currently vacant.

Description of Applicant's Request
The applicant is seeking a rezone and conditional use permit for a 91-unit planned residential development comprised of five multi-family buildings and a clubhouse.

2. Land Use

Description and Character of Surrounding Area
The property is located east of the Vista Village shopping center, north of Kootenai Street, and west of the railroad tracks that run parallel to Federal Way. The surrounding neighborhood is comprised of a mix of single-family and smaller multi-family uses.

Adjacent Land Uses and Zoning

North:	Railroad Tracks then Residential / R-1C (Single Family Residential)
South:	Railroad Tracks then Residential / R-1C (Single Family Residential) and R-3D (High Density Residential with Design Review)
East:	Railroad Tracks then Office and Residential / L-OD (Limited Office with Design Review)
West:	Single-Family Homes then Vista Village Shopping Center / R-1C (Single Family Residential) and C-2D (General Commercial with Design Review)

Site Characteristics
The site consists of a triangular shaped lot surrounded by railroad tracks on all sides. The primary access will be from Robert Street and a secondary access will be from Kootenai Street.

Special Considerations
None

3. Project Proposal

Site Design

Land Use	
Percentage of the site devoted to building coverage:	11%
Percentage of the site devoted to paving:	22%
Percentage of the site devoted to landscaping:	44%
Percentage of the site devoted to other uses (sidewalks, sports courts, railroad tracks)	23%
TOTAL	100%

Parking

Proposed		Required	
Total parking spaces proposed:	143	Total parking spaces required:	119
Accessible spaces proposed:	5	Accessible spaces required:	3
Number of compact spaces proposed:	48	Number of compact spaces	48
Bicycle parking spaces proposed:	93	Bicycle parking spaces required:	91
Parking Reduction requested?	No	Off-site Parking requested?	No

This project includes 36 one-bedroom units, 39 two-bedroom units, and 16 three-bedroom units. The Zoning Ordinance requires 1 space per one-bedroom unit, 1.25 spaces per two-bedroom unit, and 1.5 spaces per three-bedroom unit. In addition, one space per 10 dwelling units is required for guest parking. As such, a minimum of 119 parking spaces are required with a maximum allowed of 179. The applicant has provided 143 parking spaces, including 48 compact spaces and 5 accessible spaces. This project also requires 91 bicycle parking spaces, with a minimum of 23 covered. The applicant is providing 93 bicycle parking spaces. As a recommended condition of approval, a minimum of 23 bicycle parking spaces shall be covered.

Setbacks (Perimeter)

Yard	Building / Parking Required	Building / Parking Proposed
Front (Robert Street)	15' (Building) / 20' (Parking)	200' (Building) / 100' (Parking)
Side (north and south)	5' (Building) / 5' (Parking)	70'+ (Building) / 10' (Parking)
Rear (east)	15' (Building) / 15' (Parking)	70'+ (Building) / 62' (Parking)

This development complies with the transitional setback requirements for multi-family projects abutting existing single-family homes and/or residentially zoned property. Single-family homes on land zoned R-1C exist to the north and south. Multi-family buildings are not allowed to exceed the height of adjacent dwellings by more than one story or 12' located within 15' of the shared property line. The applicant proposes to setback all buildings from the adjacent properties a minimum of 70'.

Structure(s) Design

Number and Proposed Use of Buildings
5 multi-family buildings and 1 clubhouse
Building Height
33'10"
Number of Stories
2-3 stories

In accordance with the requirements of the *Citywide Design Standards and Guidelines*, parking will be located internal to the site with appropriate screening of service drives. The buildings themselves include modulation in façade and rooflines to reduce their perceived mass, as well as appropriate materials, openings and covered entries, consistent with the surrounding neighborhood. The proposed design also focuses taller, 3-story buildings at the center of the site, stepping down to 2-story buildings along the edges of the site adjacent to areas with single-family homes.

Amenities

This project includes two amenities, as required per the Zoning Ordinance. These have been provided in the form of a clubhouse with a 24-hour fitness facility, an outdoor plaza space, sport court, garden area, and multiple landscaped areas with benches, picnic tables, and walking paths. Each unit will also have a private outdoor space in the form of a balcony or patio.

4. Zoning Ordinance

Section	Description
11-03-04.3	Rezone
11-03-04.7	Planned Developments
11-04-03.1	General Purpose of Residential Districts
11-06-03.2	Multi-Family Living Uses
11-07-03	Off-Street Parking and Loading Standards
11-07-06.05	Planned Unit Development Standards

5. Comprehensive Plan

Chapter	Goals, Objectives & Policies
Chapter 2-Citywide Vision And Policies	Principle NAC7.1 Principle CC1.1 Principle CC5.1(c)
Chapter 3-Community Structure And Design	Principle GDP-N.3(a) and (b) Principle IDP-C.1 Principle IDP-N.1(a)
Chapter 4: Central Bench Planning Area Policies	Principle CB-CCN3.2 Principle CB-CCN3.4(a)
<i>Depot Bench Neighborhood Plan</i>	Land Use Goal 2.3

- Neighborhoods and Activity Centers (NAC)
- Connected Community (CC)
- General Design Principles for Neighborhoods (GDP-N)
- Infill Design Principles for Corridors (IDP-C)
- Infill Design Principles for Neighborhoods (IDP-N)
- Central Bench Policies for Centers, Corridors, and Neighborhoods (CB-CCN)

6. Transportation Data

Roadway	Frontage	Functional Classification	PM Peak Hour Traffic Count	PM Peak Level of Service
Robert Street* (North of Kootenai Street)	310'	Local	N/A	N/A
Kootenai Street** (West of Federal Way)	20'	Minor Arterial	663	"E"
Kootenai Street** (East of Vista Avenue)	0'	Minor Arterial	562	"E"
Vista Avenue***	0'	Principal Arterial	1,001	"E"

* ACHD does not set level of service thresholds for Local roadways.

** The acceptable level of service for a minor arterial is 575 vehicles per hour for two lanes and 720 vehicles per hour for three lanes.

***The acceptable level of service for a principal arterial is 1,780 vehicles per hour.

This development is estimated to generate 605 vehicle trips per day, with 56 in the PM peak hour, based on the Institute of Transportation Engineers Trip Generation Manual, 9th edition.

7. Analysis & Findings

The applicant is requesting approval of a rezone and conditional use permit for a 91-unit planned residential development. The project is located east of the Vista Village shopping center, north of Kootenai Street, and west of the railroad tracks that run parallel to Federal Way. The surrounding neighborhood is comprised of a mix of single-family and smaller multi-family uses.

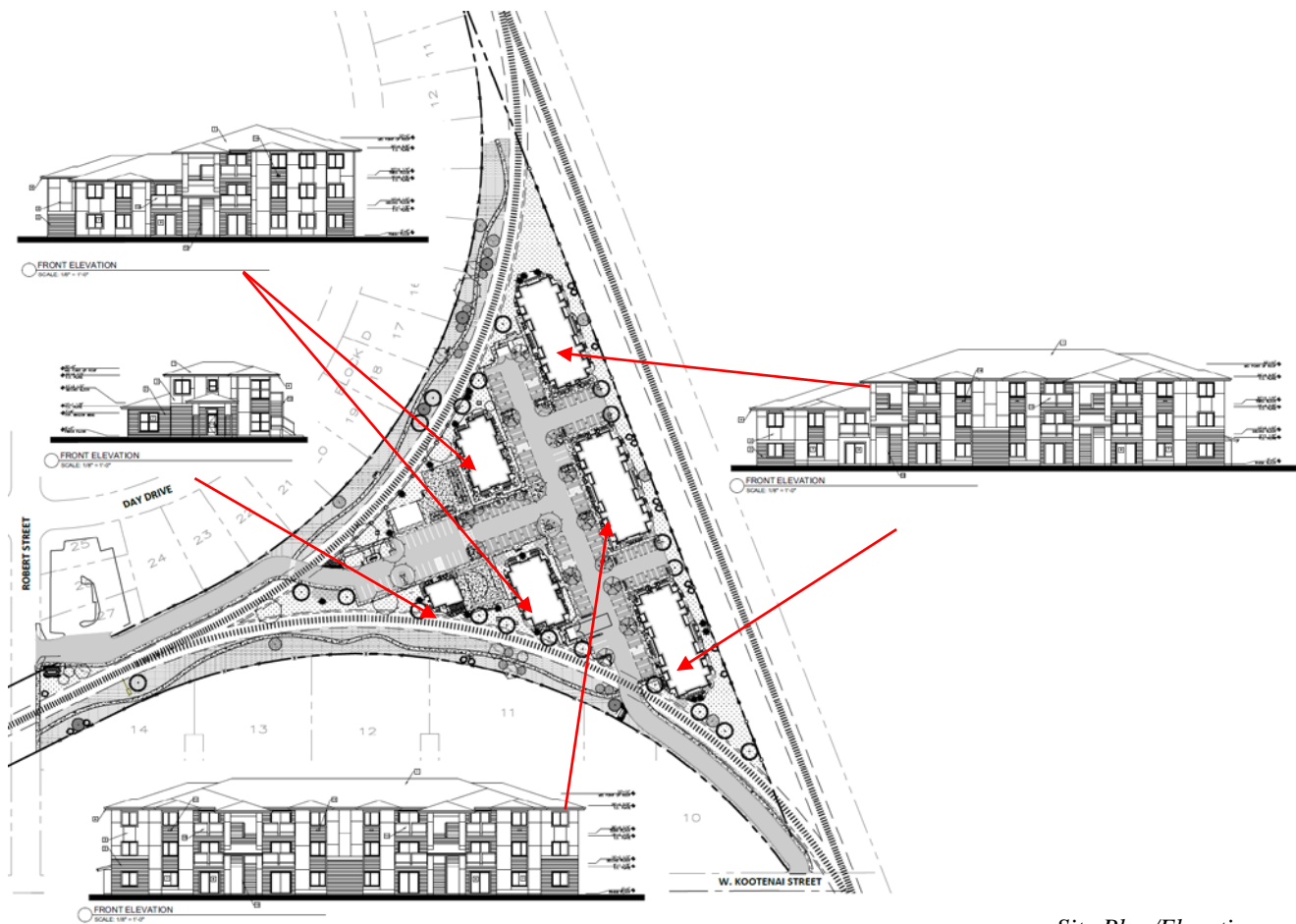


Vicinity Map

The property is currently zoned R-1C (Single Family Residential), which has a maximum residential density of eight units per acre. To accommodate the density of the project (12.3 units per acre), the applicant is requesting R-2D (Medium Density Residential with Design Review) zoning, which allows up to 14.5 units per acre. The property is designated "Compact" on the Land Use Map. The change in zoning will accommodate additional residents that can take advantage of existing services, infrastructure, and amenities in the area. In addition to the R-2D zone, A-1, A-2, R-1A, R-1B, R-1C, L-O, and N-O, R-1M, and PC are all permissible implementing zones in this designation. The rezone will maintain and preserve the compatibility of surrounding development and zoning, which is a

mixture of R-1C as well as a number of more intense zones, including R-3D (Multi-Family Residential with Design Review) to the north, L-OD (Limited Office with Design Review) to the east, and C-2D (General Commercial with Design Review) to the west.

The associated 91-unit planned residential development is comprised of five multi-family buildings and a club house. In accordance with the requirements of the *Citywide Design Standards and Guidelines*, parking will be located internal to the site with appropriate screening of service drives. The buildings themselves include modulation in façade and rooflines to reduce their perceived mass, as well as appropriate materials, openings and covered entries, consistent with the surrounding neighborhood. The proposed design also focuses taller, 3-story buildings at the center of the site, stepping down to 2-story buildings along the edges of the site adjacent to areas with single-family homes. All buildings within the development will be setback more than 70' from adjacent properties.



Site Plan/Elevations

Connectivity

A connectivity index review was not provided because there are no new streets proposed with this development. The primary access will be from Robert Street and a secondary access will be from Kootenai Street. A robust network of sidewalks and pathways will also be provided throughout the development, which will allow residents to easily access Robert Street and the commercial uses along Vista Avenue, as well as Kootenai Street. Access to transit is also readily available, with bus stops for two different routes less than ¼ mile away at the intersection of Vista Avenue and Kootenai Street to the west and Federal Way and Kootenai Street to the east.

FINDINGS**Section 11-03-04.03(C7) Rezone**

Any recommendation of the PZC shall be in writing and shall specify that the rezone meets the following criteria:

i. Is in compliance with the Comprehensive Plan.

The rezone is consistent with the Comprehensive Plan. The property is designated “Compact” on the Land Use Map. R-2D is a permissible implementing zone in this designation. The A-1, A-2, R-1A, R-1B, R-1C, L-O, and N-O, R-1M, and PC zones are also permissible implementing zones in this designation. The majority of these residential zones (A-1, A-2, R-1A, R-1B, and R-1C) would not support the density of this project. The R-1M zone, which is also an option, would support the density of the project, but is intended more for townhome development versus larger-scale multi-family development. The office zones (N-O and L-O) would be inappropriate due to the much higher density (43.5 dwellings per acre) each allows. Finally, the lack of a commercial component for this project would fail to meet the intent of the PC zone.

ii. Is in the best interests of the public convenience and welfare.

The rezone is also in the best interest of the public. The property is currently zoned R-1C (Single Family Residential), which has a maximum residential density of eight units per acre. To accommodate the density of the project (12.3 units per acre), the applicant is requesting R-2D (Medium Density Residential with Design Review) zoning, which allows up to 14.5 units per acre. The change in zoning will accommodate new residents that can make use of existing services, infrastructure, and amenities in the area.

iii. Maintains and preserves compatibility of surrounding zoning and development.

The rezone to R-2D will maintain and preserve the compatibility of surrounding zoning and development. In addition to R-1C zoning, the subject property is adjacent to a number of more intense zones, including R-3D (Multi-Family Residential with Design Review) to the north, L-OD (Limited Office with Design Review) to the east, and C-2D (General Commercial with Design Review) to the west.

Section 11-03-04.07 (C7) Planned Unit Development

The Hearing Examiner or the PZC shall review pursuant to Section 11-03-03.4 and according to the following criteria:

i. The location is compatible to other uses in the general neighborhood;

The Planning Team finds the development compatible with the adjacent neighborhood, and those reasonably expected to develop. To the west is the Vista Village Shopping Center and the commercial corridor of Vista Avenue. The remainder of the surrounding neighborhood is comprised of a mix of single-family and smaller multi-family uses. There are also a number of vacant parcels with more intense zoning that could support smaller infill projects.

ii. *The proposed use will not place an undue burden on transportation and other public facilities in the vicinity;*

Correspondence received from commenting agencies indicate the proposed use will not place an undue burden on transportation or other public services. The Ada County Highway District (ACHD) approved the project on October 17, 2016. They estimated the project will generate 605 vehicle trips per day, with 56 during the PM peak hour. As demonstrated in the attached comments, no public agency has voiced opposition to this request. The requirements of each have been included as conditions of approval.

iii. *The site is large enough to accommodate the proposed use and all yards, open spaces, pathways, walls and fences, parking, loading, landscaping and such other features as are required by this Code.*

The site is large enough to accommodate the proposed use and all required elements of a planned residential development. The applicant has proposed a design that focuses taller, 3-story buildings at the center of the site, stepping down to 2-story buildings along the edges of the site adjacent to areas with single-family homes. All buildings within the development will be setback more than 70' from adjacent properties. Proposed service drives and parking meet the requirements of Boise City Code, and the density does not exceed the limitations of the underlying zone.

iv. *The proposed use, if it complies with all conditions imposed, will not adversely affect other property of the vicinity.*

With the attached conditions of approval, the project will not adversely affect other property in the vicinity. Due to setbacks, buffering, and separation, no conflicts in terms of privacy or noise can be identified. Additionally, the project will not generate impacts associated with parking since it will provide ample parking on-site for each dwelling unit and guests.

v. *The proposed use is in compliance with the Comprehensive Plan;*

The project is consistent with the Comprehensive Plan and the *Depot Bench Neighborhood Plan*. Multiple principles within the Comprehensive Plan encourage a mix of housing in neighborhoods and infill development that does not require the costly extension of infrastructure. All necessary utilities and infrastructure are readily available to the site. The location of this project adjacent to existing railroad right-of-way also aligns with *Principle CC5.1(c)* which seeks to establish a future multi-modal transportation system that includes light rail in the Treasure Valley. *Principle IDP-N.1(a)* of *Blueprint Boise* encourages transitions in building heights and the use of variations in side yard setbacks to ensure infill development in established neighborhoods is compatible. The applicant has proposed a design that focuses taller, 3-story buildings at the center of the site, stepping down to 2-story buildings along the edges of the site adjacent to areas with single-family homes. All buildings within the development will be setback more than 70' from adjacent properties. Finally, the *Depot Bench Neighborhood Plan* identifies the subject property as a target area for medium-density housing projects.

- vi. *A multi-family building (any building containing more than two residential units) is designed to comply with the Citywide Design Standards and Guidelines;*

The project complies with many of the *Citywide Design Standards and Guidelines* as submitted. Parking will be located internal to the site with appropriate screening of service drives. The buildings themselves include modulation in façade and rooflines to reduce their perceived mass, as well as appropriate materials, openings and covered entries, consistent with the surrounding neighborhood. Upon approval of the requested rezone, this project will be subject to Design Review, which will ensure compliance with these guidelines.

8. Recommended Conditions of Approval

Site Specific

1. Compliance with plans and specifications submitted to and on file in the Planning and Development Services Department dated received **September 27, 2016**, except as expressly modified by Design Review or the following conditions:
2. **Planned Unit Development:**
 - a. All structures, parking, and service drives shall meet the setback requirements of the R-2D zone.
 - b. A minimum of 23 bicycle parking spaces shall be covered.
 - c. Written approval of the development from the impacted railroad companies is required prior to issuance of any permits.

Agency Requirements

3. Comply with requirements of the Ada County Highway District (ACHD) as outlined in their approval dated **October 17, 2016**.
4. Comply with the requirements of the Boise City Public Works Department (BCPW) for, Sewer, Solid Waste, Drainage, and Street Lights per memos dated **September 29-30, 2016**. Please contact BCPW at 384-3992.
5. Comply with the requirements of the Boise Fire Department as outlined in their approval dated **October 10, 2016**.
6. Comply with requirements of Central District Health Department.
7. Comply with the requirements of the Boise School District as outlined in comments dated **October 10, 2016**.

Standard Conditions of Approval

8. Building permit approval is contingent upon the determination that the site is in conformance with the Boise City Subdivision Ordinance. Contact Planning and Development Services at (208) 384-3830 regarding questions pertaining to this condition.
9. All landscaping areas shall be provided with an underground irrigation system. Landscaping shall be maintained according to current accepted industry standards to promote good plant health, and any dead or diseased plants shall be replaced. All landscape areas with shrubs shall have approved mulch, such as bark or soil aid.
10. Swales/retention/detention areas shall not be located along the streets, unless it can be shown that landscaped berms/shrubs will screen the swales.
11. In compliance with Title 9, Chapter 16, Boise City Code, anyone planting, pruning, removing or trenching/excavating near any tree(s) on ACHD or State right-of-ways must obtain a permit from Boise City Community Forestry at least one (1) week in advance of such work by calling (208) 384-4083. Species shall be selected from the Boise City Tree Selection Guide.
12. Deciduous trees shall be not less than 2" to 2 1/2" inch caliper size at the time of planting, evergreen trees 5' to 6' in height, and shrubs 1 to 5 gallons, as approved by staff. All plants are to conform to the American Association of Nurseryman Standards in terms of size and quality.
13. Utility services shall be installed underground.
14. An occupancy permit will not be issued by the Planning and Development Services Department until all of these conditions have been met. In the event a condition(s) cannot be met by the desired date of occupancy, the Planning Director will determine whether the condition(s) is bondable or should be completed, and if determined to be bondable, a bond or other surety acceptable to Boise City will be required in the amount of 110% of the value of the condition(s) that is incomplete.
15. All amenities, landscaping, fencing, sidewalks and underground irrigation shall be installed or bonded for prior to the issuance of a building permit. For bonding, the applicant is required to provide a minimum of two bids for the amenities, landscaping materials and the installation. The bond shall be for 110% of the highest bid and submitted to the Subdivision desk on the 2nd floor of City Hall. For additional information, please call (208) 384-3830.
16. No change in the terms and conditions of this approval shall be valid unless in writing and signed by the applicant or his authorized representative and an authorized representative of Boise City. The burden shall be upon the applicant to obtain the written confirmation of any change and not upon Boise City.
17. Any change by the applicant in the planned use of the property, which is the subject of this application, shall require the applicant to comply with all rules, regulations, ordinances, plans, or other regulatory and legal restrictions in force at the time the applicant, or successors of interest, advise Boise City of intent to change the planned use of the property described herein, unless a variance in said requirements or other legal relief is granted pursuant to the law in effect at the time the change in use is sought.

18. Failure to abide by any condition of this conditional use permit shall be grounds for revocation by the Boise City Planning and Zoning Commission.
19. This conditional use permit shall be valid for a period not to exceed twenty four (24) months from the date of approval by the Planning and Zoning Commission. Within this period, the holder of the permit must acquire construction permits and commence placement of permanent footings and structures on or in the ground. The definition of structures in this context shall include sewer lines, water lines, or building foundations.
20. Prior to the expiration of this conditional use, the Commission may, upon written request by the holder, grant a two-year time extension. A maximum of two (2) extensions may be granted.
21. To reduce the noise impact of construction on nearby residential properties, all exterior construction activities shall be limited to the hours between 7:00 a.m. and 7:00 p.m. Monday through Friday and 8:00 a.m. to 6:00 p.m. for Saturday and Sunday. Low noise impact activities such as surveying, layout and weather protection may be performed at any time. After each floor of the structure or building is enclosed with exterior walls and windows, interior construction of the enclosed floors can be performed at any time.



Sept 27, 2016

City of Boise Planning & Development Services
150 N. Capitol Blvd
Boise, ID 83701-0500

Re: Planned Unit Development and Rezone Applications for the Station Village Apartments

The proposed applications are for a 7.39 acre multi-family project. It will include 91 dwelling units, related buildings and supporting infrastructure. The project is located on two parcels near the Vista and Federal Way corridors – 2350 W. Kootenai St. & 1110 S. Robert Street. The property is directly east of the existing Vista Village Retail Center. Both parcels have a current R-1C zoning designation.

We are submitting applications for a Rezone to request an R-2D designation as well as a Planned Unit Development to accommodate the proposed density.

We appreciate the time and cooperation received from the planning staff during the planning and design stages of this project and look forward to a continued partnership in seeing this project come to fruition. We have likewise communicated with the Fire Department, Boise Valley Railroad, ACHD & the Depot Bench Neighborhood Association for additional comment. This has been helpful as we've made several revisions to the initial layout.

This project is in an area rich with character and established neighborhoods. The location has close access to many services, amenities and institutions in the immediate vicinity and near vicinity, including downtown, the airport, city parks, the Boise River, greenbelt, BSU campus, and other local schools. In addition the property affords great views of downtown and the foothills.

The introduction of higher density housing in this area aligns with the Comprehensive Planning associated with the Central Bench area and specifically the Vista Avenue Corridor. Much of the local services are in place to support the density. This type of density will also encourage new and future rehabilitation of existing services along this corridor and in the area. The proposed housing will offer a specific housing choice in an area that is mostly comprised of single family detached homes.

The owners of the land have a vested interest in the current and future well-being of the neighborhood. It is their goal to provide high quality multi-family living that attracts diverse residents and helps to strengthen the existing neighborhood. As the current long time owners of the Vista Village Retail Center, they have shown their commitment to neighbors and tenants in recent façade and landscape enhancements to their buildings. The construction of this type of product will be an extension of this progress.

DAVE EVANS CONSTRUCTION

7761 W. Riverside Dr. Suite 100, Boise, Idaho 83714 • (208) 853-1203 • Fax (208) 853-1220
www.devansconstruction.com

The property is unique in its configuration and also bordering conditions. The triangular shaped parcels are bordered on the north by smaller single family residences on Day Drive, to the south by mostly single family residences, and the Union Pacific Railroad Tracks and ROW to the east.

One parcel has frontage along Robert Street to the west, which will serve as the main entrance. The other parcel has limited frontage along Kootenai Street to the south. At this time we are proposing limited access along this frontage with the intention of a full service access that will align with Columbus Street. The property owner is currently in discussion with the adjacent property owner for potential future arrangement. Cooperation of both owners will be required for this alignment.

Inset by 60' along both the north and south boundaries is a cross access easement with Boise Valley Railroad. Railroad spurs run within these easements facilitating the occasional short term storage of railroad cars. The original use of these spurs was for reversing the course of trains, although this is no longer used for such. This condition requires the cooperation of the railroad in our proposed construction and improvements. For some time the owner has been in contact and most recently received approval from Boise Valley Railroad for the two vehicle and pedestrian crossings shown in the attached site plan. The safety of future residents is of utmost concern in contemplating this project. Open fencing between the buildings & outdoor amenities and the spurs will help create a border. In addition the railroad will maintain a 25' wide area, centered on the tracks clearly delineated. Additional information is available as to the frequency and duration of railroad use if staff or the Commission requires.

The project includes a total of (5) apartment buildings. These vary in size and configuration. Included within these buildings are 1 bedroom, 2 bedroom and 3 bedroom units. The total number of residential units is (91). This results in a proposed density of 12.31 units/acre. The allowed density for an R2 district is 14.5. Another two story structure houses a clubhouse, manager's office, 24 hour fitness facility, maintenance storage, and a manager's apartment on the second floor.

Additional project amenities include an outdoor plaza space, sports court, garden area, numerous open landscaped areas with park benches and picnic tables, and walking paths along the north and south property boundaries. Each unit has a dedicated private outdoor space with a patio at grade or balcony for units on the second and third floors.

Attention has been paid to internal pedestrian paths and connections as well as connections to the adjacent existing neighborhood streets. This internal network is enhanced with multiple outdoor public seating and activity areas, and ample landscaping.

After meeting with the neighbors and becoming aware of their concerns related to views, transition and privacy we've revised the configuration and heights of the residential buildings. Buildings A, D and E, per the attached site plan and elevations, step down to two stories on the ends bordering these adjacent properties of single family residences. The area containing the spurs and walking paths also create a sizeable buffer area against the existing single family properties. The architectural style, initially more contemporary in material and building form now is shown with more traditional roof lines.

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The proposed schematic architectural elevations and plans for the buildings are consistent with the city's design guidelines. All of the buildings employ a variety of materials, multiple reliefs and jogs in the walls to provide architectural character and interest. Air conditioning condensing units for each building will be placed on the ground, but screened with fencing and landscaping. All new proposed utilities will be placed underground.

Please refer to the attached submitted materials for additional information. Thank you for your time and consideration. Please feel free to contact me with any questions.

A handwritten signature in black ink, appearing to read 'Rob Powell', with a long, sweeping horizontal stroke extending to the right.

Rob Powell/Architect - Dave Evans Construction

DAVE EVANS CONSTRUCTION

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LANDSCAPE NOTES

1. VERIFICATION OF TOTAL LANDSCAPE MATERIAL QUANTITIES AS SHOWN ON THE LANDSCAPE PLANS AND IN THE PLANT SCHEDULE SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR. THE LANDSCAPE ARCHITECT SHALL BE NOTIFIED OF ANY DISCREPANCIES PRIOR TO FINAL BIDDING OR INSTALLATION.
2. CONTRACTOR SHALL REPORT TO THE LANDSCAPE ARCHITECT ANY DISCREPANCIES OR CONDITIONS WHICH WILL PREVENT THE PROPER EXECUTION OF THIS WORK PRIOR TO BEGINNING WORK.
3. CONTRACTOR SHALL LOCATE AND IDENTIFY EXISTING UNDERGROUND AND OVERHEAD UTILITIES WITHIN CONTRACT WORK AREAS PRIOR TO CONSTRUCTION. CONTACT DIG LINE, INC. @ 1.800.342.1885. PROVIDE ADEQUATE MEANS OF PROTECTION OF UTILITIES AND SERVICES DESIGNATED TO REMAIN. REPAIR UTILITIES DAMAGED DURING SITE WORK OPERATIONS AT CONTRACTOR'S EXPENSE.
4. ALL PLANT MATERIAL SHALL CONFORM TO THE AMERICAN NURSERYMAN STANDARDS FOR TYPE AND SIZE SHOWN. PLANTS WILL BE REJECTED IF NOT IN HEALTHY GROWING CONDITION AND/OR NOT AT THE SIZE SPECIFIED.
5. ALL PLANT MATERIAL SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR BEGINNING AT THE DATE OF ACCEPTANCE BY OWNER. REPLACE ALL PLANT MATERIAL FOUND DEAD OR NOT IN HEALTHY CONDITION IMMEDIATELY WITH COMPARABLE SIZE AND SPECIES AT NO COST TO THE OWNER.
6. IMMEDIATELY CLEAN UP ANY TOPSOIL OR OTHER DEBRIS ON THE SITE CREATED FROM LANDSCAPE OPERATIONS AND DISPOSE OF PROPERLY OFF-SITE.
7. REPAIR AND/OR REPLACE ANY EXISTING LANDSCAPE AREAS WITHIN THE WORK LIMITS OF THE PROJECT THAT HAVE BEEN DAMAGED OR REMOVED IN THE COURSE OF THE COMPLETION OF THE PROJECT, INCLUDING WORK DONE BY OTHERS THAT IS SHOWN IN THE CONTRACTED PLAN SET. MATCH ALL MATERIALS REPAIRED OR REPLACED TO ENSURE CONSISTENCY.
8. TREES SHALL NOT BE PLANTED WITHIN A MINIMUM OF 5' OF ANY SUEZ WATER SERVICE.
9. PROPOSED PLANT MATERIAL SHALL BE IRRIGATED BY A NEW, UNDERGROUND PRESSURE IRRIGATION SYSTEM. SOURCE OF IRRIGATION SHALL BE VIA THE PROPERTIES SURFACE WATER RIGHT (ASSUMING WATER IS AVAILABLE AND DELIVERED ON A RELIABLE SCHEDULE AND AT THE QUANTITY REQUIRED). IF THE SURFACE WATER RIGHT IS NOT SUFFICIENT OR IS NOT DELIVERED ON A REGULAR SCHEDULE, THE IRRIGATION SYSTEM WILL BE SUPPLIED WATER VIA, POTABLE, METERED SUEZ WATER COMPANY WATER.

KEY NOTES (#)

1. TRASH ENCLOSURE (SEE ARCHITECTURAL SITE PLAN).
2. PICNIC TABLE.
3. BENCH.
4. SHADE TRELLIS (SEE ARCHITECTURAL).
5. MAILBOX CLUSTER.
6. APARTMENT MONUMENT SIGNAGE.
7. RESIDENT'S GARDEN AREA.
8. SPORTS COURT.
9. BIKE RACK.

PLANT SCHEDULE

SYMBOL	QUANT.	COMMON NAME	BOTANICAL NAME	MINIMUM SIZE/COND	SIZE @ MATURITY (HT X W)	NOTES
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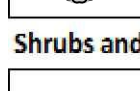
Deciduous Shade Trees

	5	Norwegian Sunset Maple	Acer truncatum x Acer platanoides 'Keithsform'	2" C al. B&B	35' x 25'	Class II
	13	Cimmeron Ash	Fraxinus pennsylvanica 'Cimmeron'	2" C al. B&B	50' x 30'	Class II
	10	Patmore Ash	Fraxinus pennsylvanica 'Patmore'	2" C al. B&B	60' x 40'	Class II
	21	Greenspire Little Leaf Linden	Tilia cordata 'Greenspire'	2" C al. B&B	40' x 30'	Class II



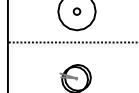

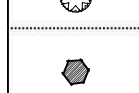
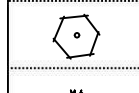
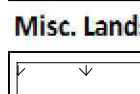
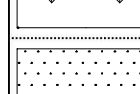
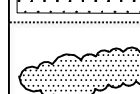
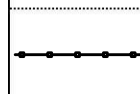
Conifers

	7	Hoopsil Spruce	Picea pungens 'Hoopsil'	7'-8' B&B	40' x 20'	Conifer
	16	Bruns Serbian Spruce	Picea omorika 'Bruns'	7'-8' B&B	30' x 10'	Conifer
	5	Colorado Blue Spruce	Picea pungens glauca	7'-8' B&B	50' x 20'	Conifer
	19	Columar Easter White Pine	Pinus strobus 'Fastigiata'	7'-8' B&B	30' x 10'	Conifer

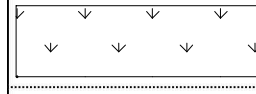
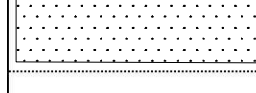
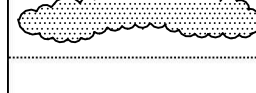
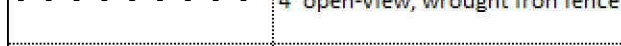
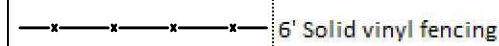
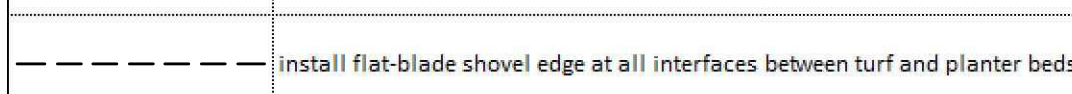
Flowering/Ornamental Trees

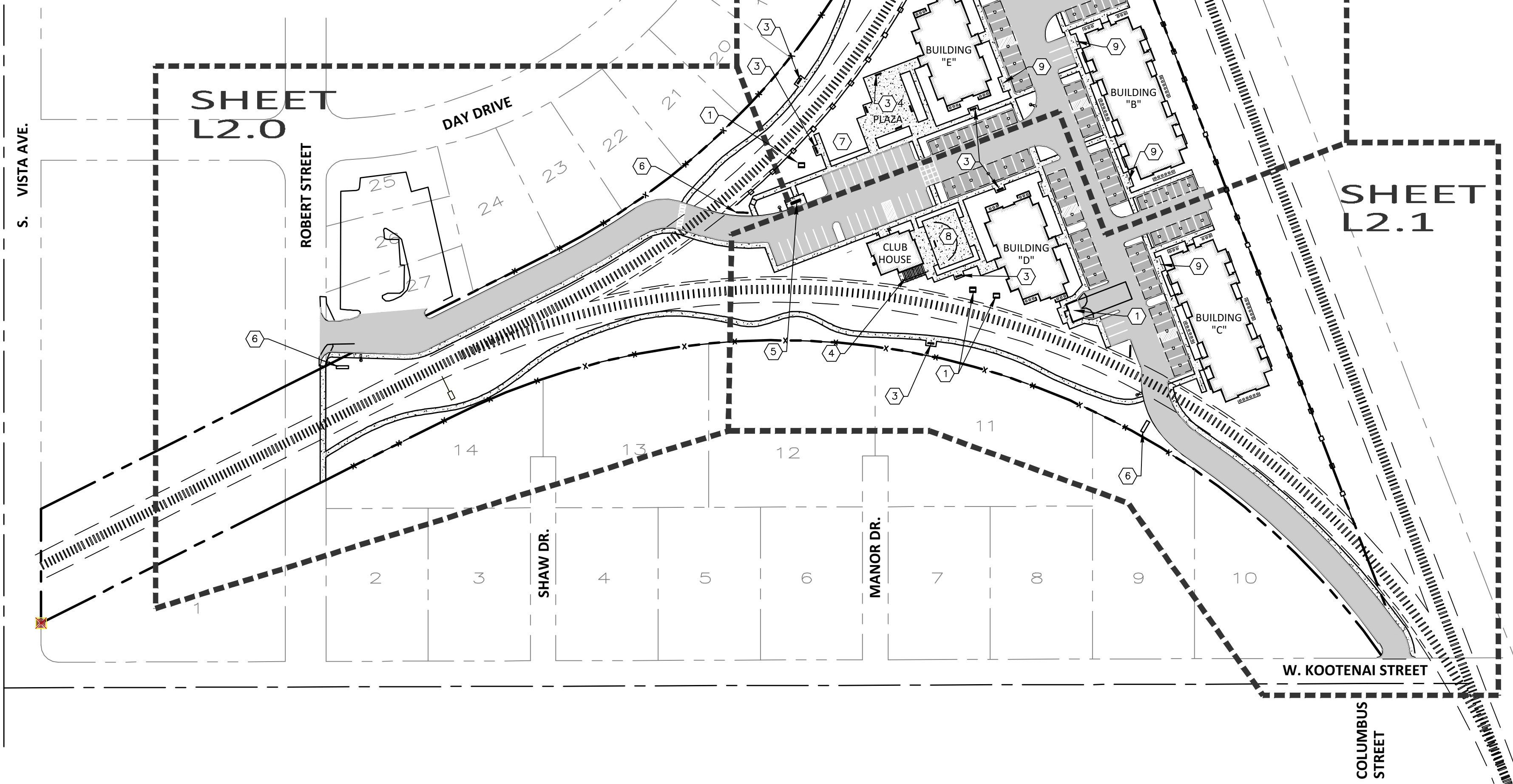
	11	Chanticleer Pear	Pyrus calleryana 'Chanticleer'	2" B&B	30' x 15'	Class III
	7	Spring Snow Crabapple	Malus x Spring Snow	2" B&B	25' x 18'	Class III
	11	Royalty Crabapple	Malus x 'Royalty'	2" B&B	20' x 20'	Class III

Shrubs and Ornamental Grasses

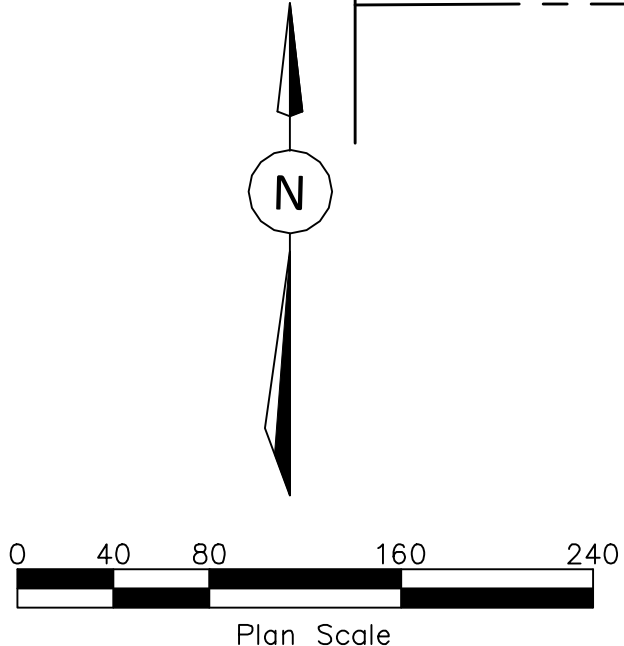
	173	Stella de Oro Daylily	Hemerocallis 'Stella de Oro'	1 Gal. Cont.	2' x 2'	Perennial
	48	Limemound Spirea	Spirea bumalda 'Limemound'	2 Gal. Cont.	3' x 4'	Shrub
	48	Ivory Halo Dogwood	Cornus alba 'Bailhalo'	2 Gal. Cont.	5' x 6'	Shrub
	17	Cistena Plum	Prunus cistena	5 Gal. Cont.	7' x 6'	Shrub
	84	Flower Carpet Rose (Red)	Rosa 'Noare'	1 Gal. Cont.	3' x 3'	Flowering Shrub
	32	Little Kitten Maiden Grass	Miscanthus sinensis 'Little Kitten'	1 Gal. Cont.	3' x 2'	Ornamental Grass
	26	Dwarf Mugo Pine	Pinus mugo pumilio	2 Gal. Cont.	4' x 6'	Evergreen Shrub
	207	Crimson Pygmy Dwarf Japanese Barberry	Berberis thunbergii 'Crimson Pygmy' ('Atropururea Nana')	1 Gal. Cont.	2' x 3'	Evergreen Shrub
	27	Dwarf Red Leaf Sand Cherry	Prunus x cistena	5 Gal. Cont.	8' x 6'	large flowering shrub
	76	Foerster's Feather Reed Grass	Calamagrostis x acutiflora 'Karl Foerster'	2 Gal. Cont.	6' x 2'	Ornamental Grass
	78	Broadmoor Juniper	Juniperus sabina 'Broadmoor'	2 Gal. Cont.	2' x 5'	evergreen groundcover

Misc. Landscape Materials

	Turf (Sod) Xerilawn, Shadowmaster (both available from The Turf Company, Meridian ID) or approved equal drought-resistant fine fescue
	Snake River Wheatgrass, Elymus wawawaiensis (seed) or approved equal, native, drought-tolerant perennial grass. These grass areas are intended to be left un-mown.
	Native, perennial seed-mix
	4' open-view, wrought iron fence
	6' Solid vinyl fencing
	Install flat-blade shovel edge at all interfaces between turf and planter beds

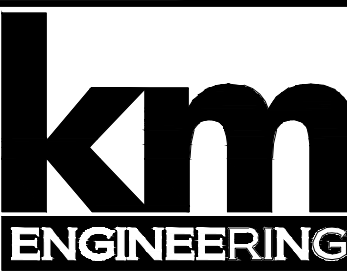


NOTE:
THESE PLANS ARE INTENDED TO SHOW THE PROPOSED LANDSCAPE PLANT MATERIALS ONLY. ALL PROPOSED GRADING, UNDERGROUND UTILITIES, STORM WATER FACILITIES, HARDSCAPE IMPROVEMENTS, AND OTHER MISCELLANEOUS SITE IMPROVEMENTS, SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPROVED CIVIL PLANS. IN THE EVENT OF A CONFLICT BETWEEN THE CIVIL PLANS AND THE LANDSCAPE PLAN, CONTRACTOR SHALL NOTIFY THE ENGINEER AND LANDSCAPE ARCHITECT IMMEDIATELY.




DEPOT VILLAGE APARTMENTS
PUD LANDSCAPE - INDEX SHEET
BOISE, IDAHO

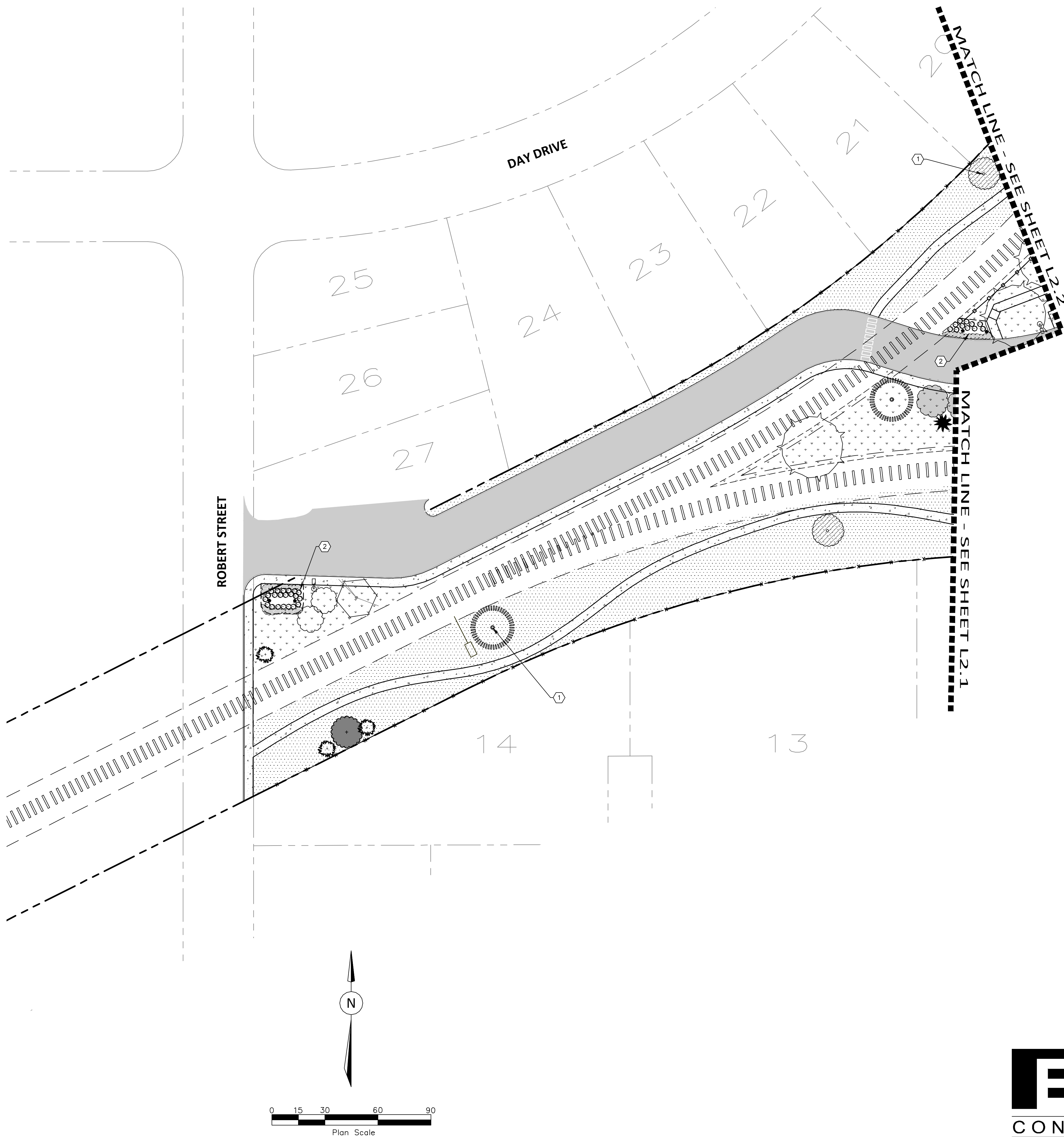
REVISIONS		
NO.	ITEM	DATE



9233 WEST STATE STREET
BOISE, IDAHO 83714
PHONE (208) 639-6939
FAX (208) 639-6930



DATE: SEPT. 2016
PROJECT: 16-070
SHEET NO.
L1.0



LANDSCAPE LEGEND

SYMBOL	COMMON NAME	NOTES
	Norwegian Sunset Maple	Class II
	Cimmeron Ash	Class II
	Patmore Ash	Class II
	Greenspire Little Leaf Linden	Class II
	Hoopsii Spruce	Conifer
	Brun's Serbian Spruce	Conifer
	Colorado Blue Spruce	Conifer
	Columnar Easter White Pine	Conifer
	Chanticleer Pear	Class III
	Spring Snow Crabapple	Class III
	Royalty Crabapple	Class III
	Stella de Oro Daylily	Perennial
	Limemound Spirea	Shrub
	Ivory Halo Dogwood	Shrub
	Cistena Plum	Shrub
	Flower Carpet Rose (Red)	Flowering Shrub
	Little Kitten Maiden Grass	Ornamental Grass
	Dwarf Mugo Pine	Evergreen Shrub
	Crimson Pygmy Dwarf Japanese Barberry	Evergreen Shrub
	Dwarf Red Leaf Sand Cherry	Large flowering shrub
	Foerster's Feather Reed Grass	Ornamental Grass
	Broadmoor Juniper	Evergreen groundcover
	Turf (Sod) Kerilawn, Shadowmaster (both available from The Turf Company, Meridian ID) or approved equal drought-resistant fine fescue	
	Snake River Wheatgrass, Elymus wawawaiensis (seed) or approved equal, native, drought-tolerant perennial grass. These grass areas are intended to be left unmown.	
	Native, perennial seed-mix	
	4' open-view, wrought iron fence	
	6' solid vinyl fencing	
	Install flat-blade shovel edge at all interfaces between turf and planter beds	

SEE SHEET L1.0 FOR COMPLETE PLANT SCHEDULE

KEY NOTES (#)

1. INSTALL 36" MIN. Ø TREE RING, WITH 3" THICKNESS SOIL AID BARK, AT ALL TREES LOCATED WITHIN TURF AREAS.
2. ALL PLANTER BEDS SHALL RECEIVE A SURFACE COURSE OF "WALK-ON" ORGANIC BARK, OR APPROVED EQUAL (MIN. 3" THICKNESS) OVER (NON-PLASTIC) WEED BARRIER FABRIC.

DEPOT VILLAGE APARTMENTS - PUD/REZONE
LANDSCAPE PLAN - WEST AREA OF SITE
BOISE, IDAHO

REVISIONS		
NO.	ITEM	DATE

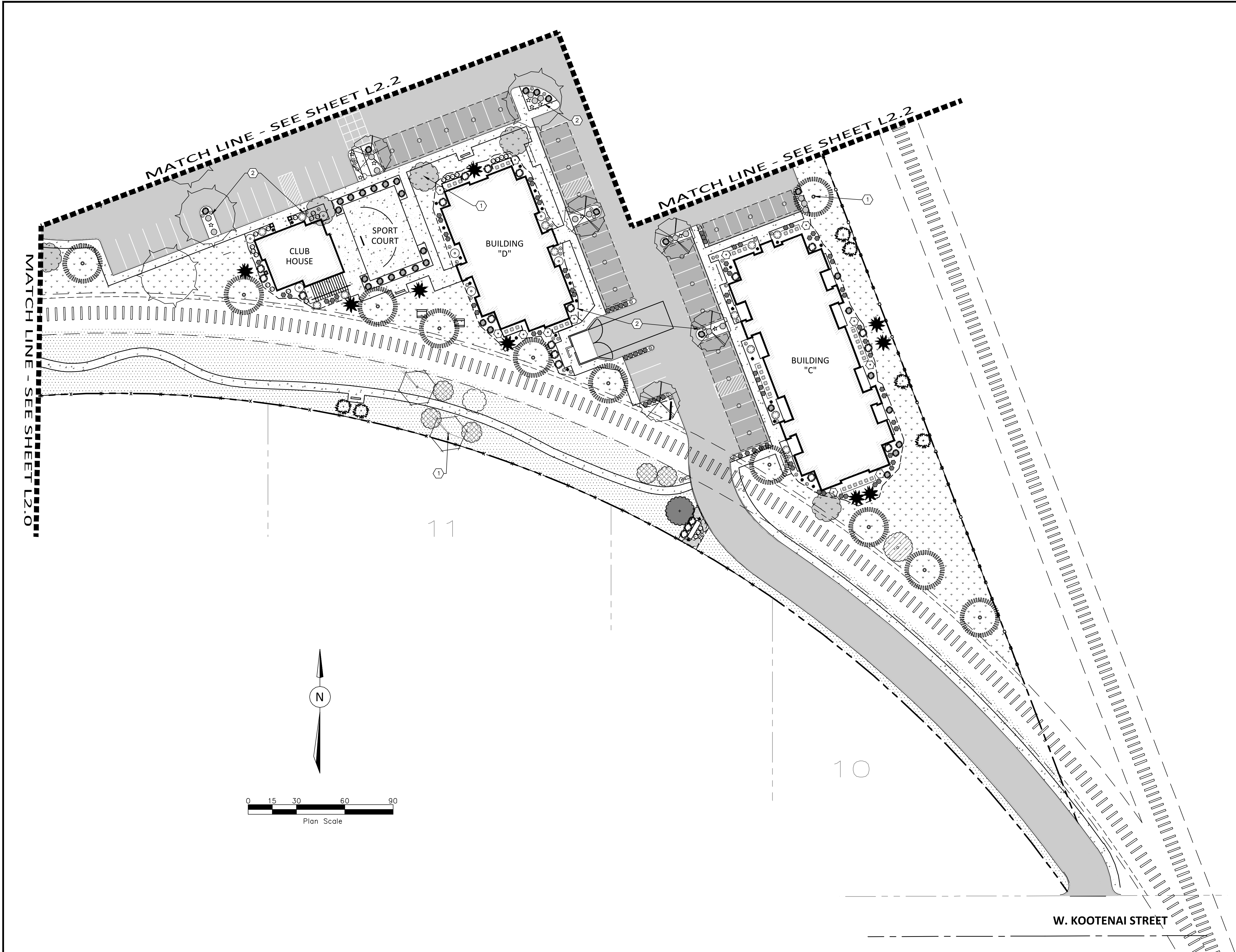
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ENGINEERING

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BOISE, IDAHO 83714
PHONE (208) 639-6939
FAX (208) 639-6930

STATE OF IDAHO
BOISE, IDAHO
9/27/16
LANDSCAPE ARCHITECT

DATE: SEPT. 2016
PROJECT: 16-070
SHEET NO. L2.0

DAVE EVANS
CONSTRUCTION



LANDSCAPE LEGEND		
SYMBOL	COMMON NAME	NOTES
	Norwegian Sunset Maple	Class II
	Cimmeron Ash	Class II
	Patmore Ash	Class II
	Greenspire Little Leaf Linden	Class II
	Hoopsii Spruce	Conifer
	Bruns Serbian Spruce	Conifer
	Colorado Blue Spruce	Conifer
	Columnar Easter White Pine	Conifer
	Chanticleer Pear	Class III
	Spring Snow Crabapple	Class III
	Royalty Crabapple	Class III
	Stella de Oro Daylilly	Perennial
	Limehound Spirea	Shrub
	Ivory Halo Dogwood	Shrub
	Cistena Plum	Shrub
	Flower Carpet Rose (Red)	Flowering Shrub
	Little Kitten Maiden Grass	Ornamental Grass
	Dwarf Mugo Pine	Evergreen Shrub
	Crimson Pygmy Dwarf Japanese Barberry	Evergreen Shrub
	Dwarf Red Leaf Sand Cherry	large flowering shrub
	Foerster's Feather Reed Grass	Ornamental Grass
	Broadmoor Juniper	evergreen groundcover
	Turf (Sod) Xerilawn, Shadowmaster (both available from The Turf Company, Meridian ID) or approved equal drought-resistant fine fescue	
	Snake River Wheatgrass, Elymus wawawaiensis (seed) or approved equal, native, drought-tolerant perennial grass. These grass areas are intended to be left unmown.	
	Native, perennial seed-mix	
	4' open-view, wrought iron fence	
	6' solid vinyl fencing	
	install flat-blade shovel edge at all interfaces between turf and planter beds	

SEE SHEET L1.0 FOR COMPLETE PLANT SCHEDULE

KEY NOTES (#)

1. INSTALL 36" MIN. Ø TREE RING, WITH 3" THICKNESS SOIL AID BARK, AT ALL TREES LOCATED WITHIN TURF AREAS.
2. ALL PLANTER BEDS SHALL RECEIVE A SURFACE COURSE OF "WALK-ON" ORGANIC BARK, OR APPROVED EQUAL (MIN. 3" THICKNESS) OVER (NON-PLASTIC) WEED BARRIER FABRIC.



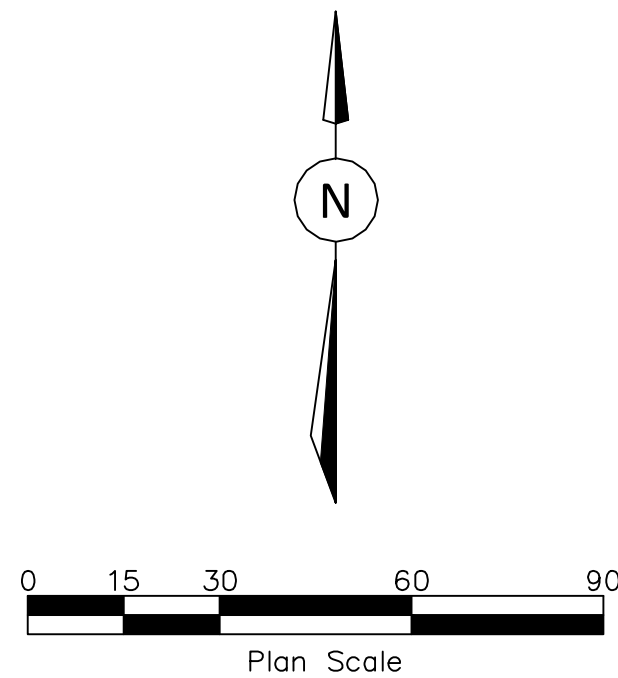
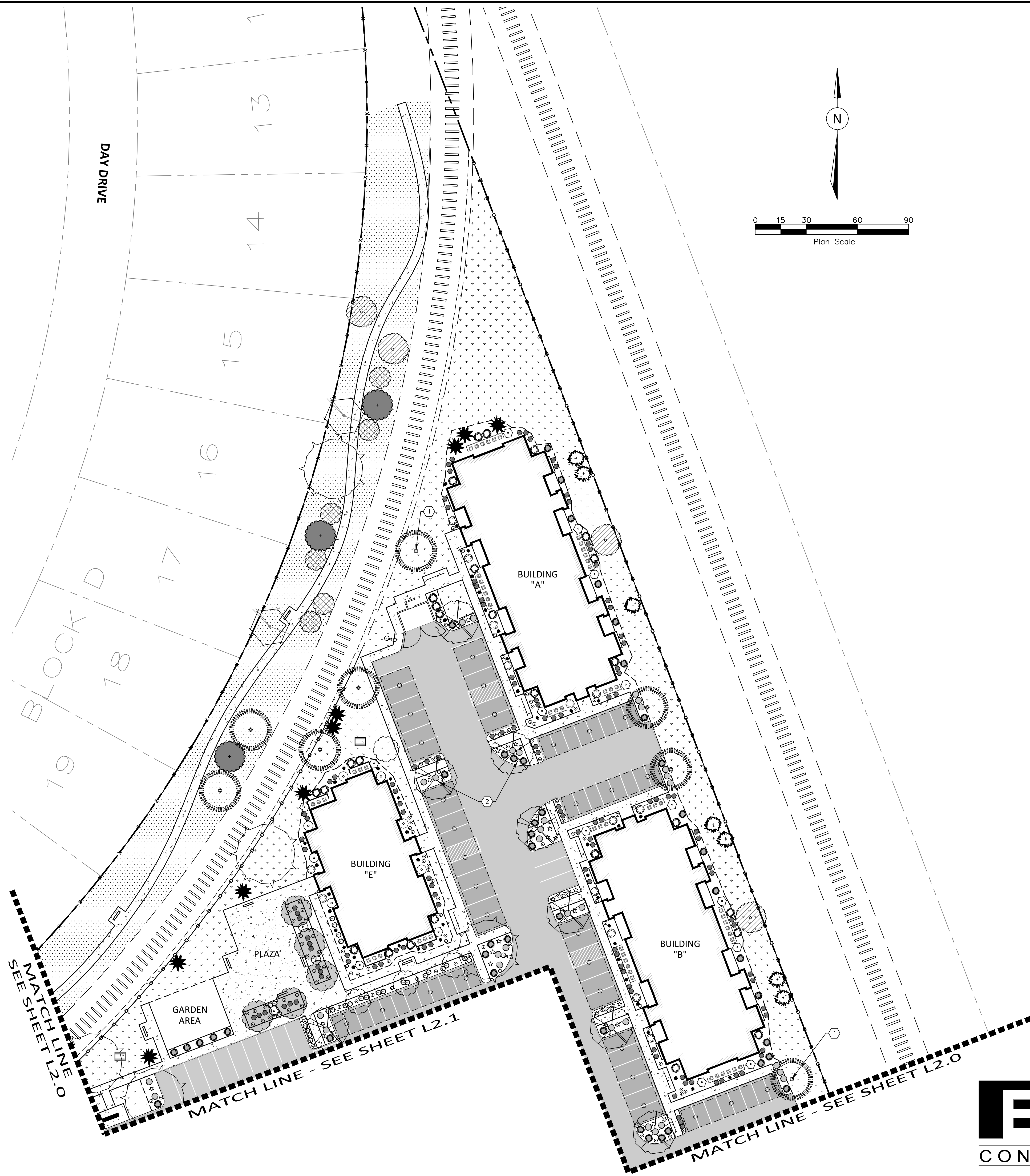
DEPOT VILLAGE APARTMENTS - PUD/REZONE
LANDSCAPE PLAN - SOUTHEAST AREA OF SITE
BOISE, IDAHO

REVISIONS		
NO.	ITEM	DATE



STATE OF IDAHO
BOISE, IDAHO
9/27/16
LANDSCAPE ARCHITECT

DATE: SEPT. 2016
PROJECT: 16-070
SHEET NO. L2.1



LANDSCAPE LEGEND		
SYMBOL	COMMON NAME	NOTES
	Norwegian Sunset Maple	Class II
	Cimmeron Ash	Class II
	Patmore Ash	Class II
	Greenspire Little Leaf Linden	Class II
	Hoopsii Spruce	Conifer
	Bruns Serbian Spruce	Conifer
	Colorado Blue Spruce	Conifer
	Columnar Easter White Pine	Conifer
	Chanticleer Pear	Class III
	Spring Snow Crabapple	Class III
	Royalty Crabapple	Class III
	Stella de Oro Daylily	Perennial
	Limemound Spirea	Shrub
	Ivory Halo Dogwood	Shrub
	Cistena Plum	Shrub
	Flower Carpet Rose (Red)	Flowering Shrub
	Little Kitten Maiden Grass	Ornamental Grass
	Dwarf Mugo Pine	Evergreen Shrub
	Crimson Pygmy Dwarf Japanese Barberry	Evergreen Shrub
	Dwarf Red Leaf Sand Cherry	large flowering shrub
	Foerster's Feather Reed Grass	Ornamental Grass
	Broadmoor Juniper	evergreen groundcover
	Turf (Sod) Xerilawn, Shadowmaster (both available from The Turf Company, Meridian ID) or approved equal drought-resistant fine fescue	
	Snake River Wheatgrass, Elymus wawawaiensis (seed) or approved equal, native, drought-tolerant perennial grass. These grass areas are intended to be left unmown.	
	Native, perennial seed-mix	
	4' open-view, wrought iron fence	
	6' Solid vinyl fencing	
	Install flat-blade shovel edge at all interfaces between turf and planter beds	

SEE SHEET L1.0 FOR COMPLETE PLANT SCHEDULE

KEY NOTES (#)

1. INSTALL 36" MIN. Ø TREE RING, WITH 3" THICKNESS SOIL AND BARK, AT ALL TREES LOCATED WITHIN TURF AREAS.
2. ALL PLANTER BEDS SHALL RECEIVE A SURFACE COURSE OF "WALK-ON" ORGANIC BARK, OR APPROVED EQUAL (MIN. 3" THICKNESS) OVER (NON-PLASTIC) WEED BARRIER FABRIC.

DEPOT VILLAGE APARTMENTS - PUD/REZONE
LANDSCAPE PLAN - NORTH AREA OF SITE
BOISE, IDAHO

REVISIONS		
NO.	ITEM	DATE

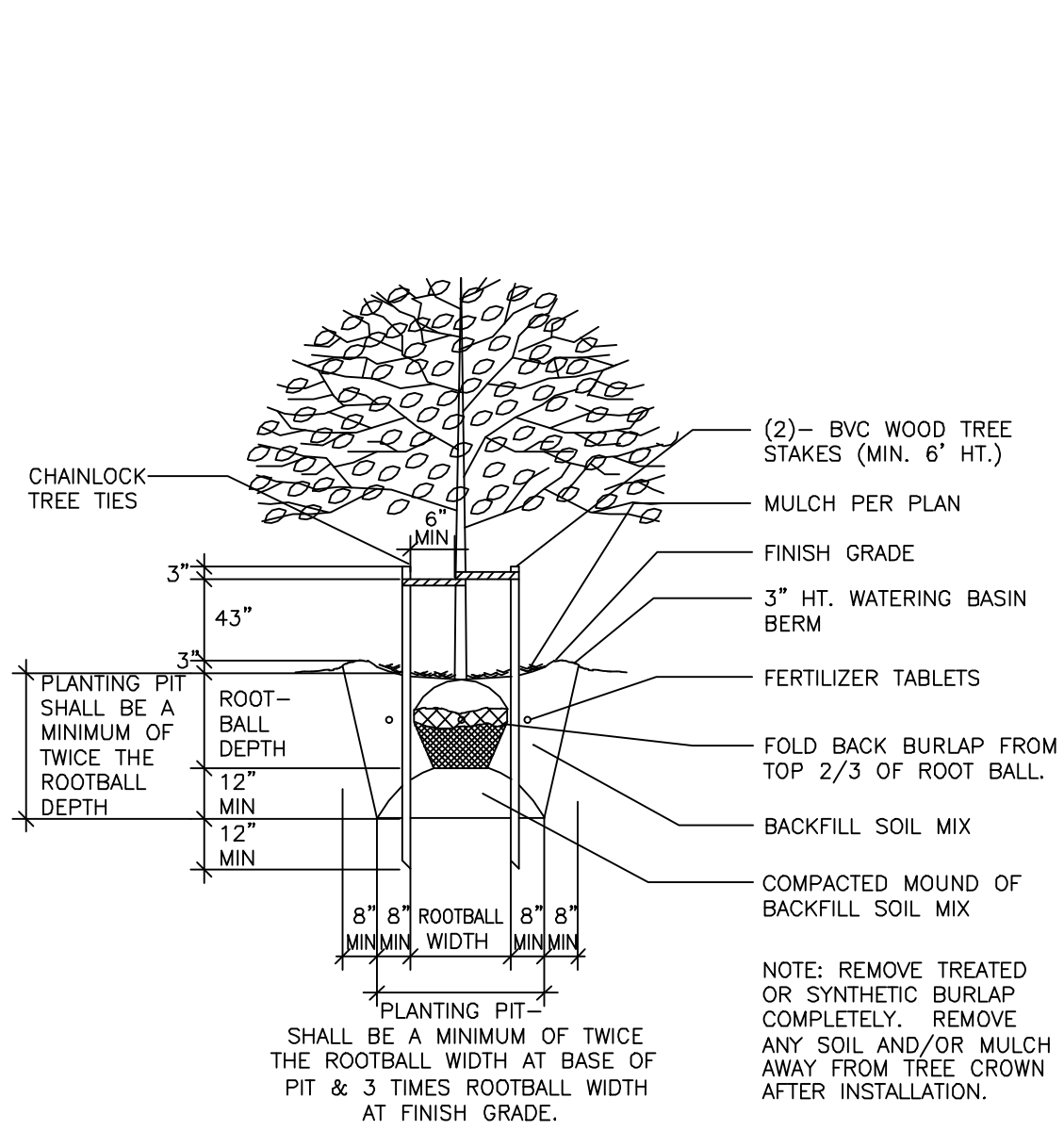
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9233 WEST STATE STREET
BOISE, IDAHO 83714
PHONE (208) 639-6939
FAX (208) 639-6930

STATE OF IDAHO
BOISE, IDAHO
9/27/16
LANDSCAPE ARCHITECT

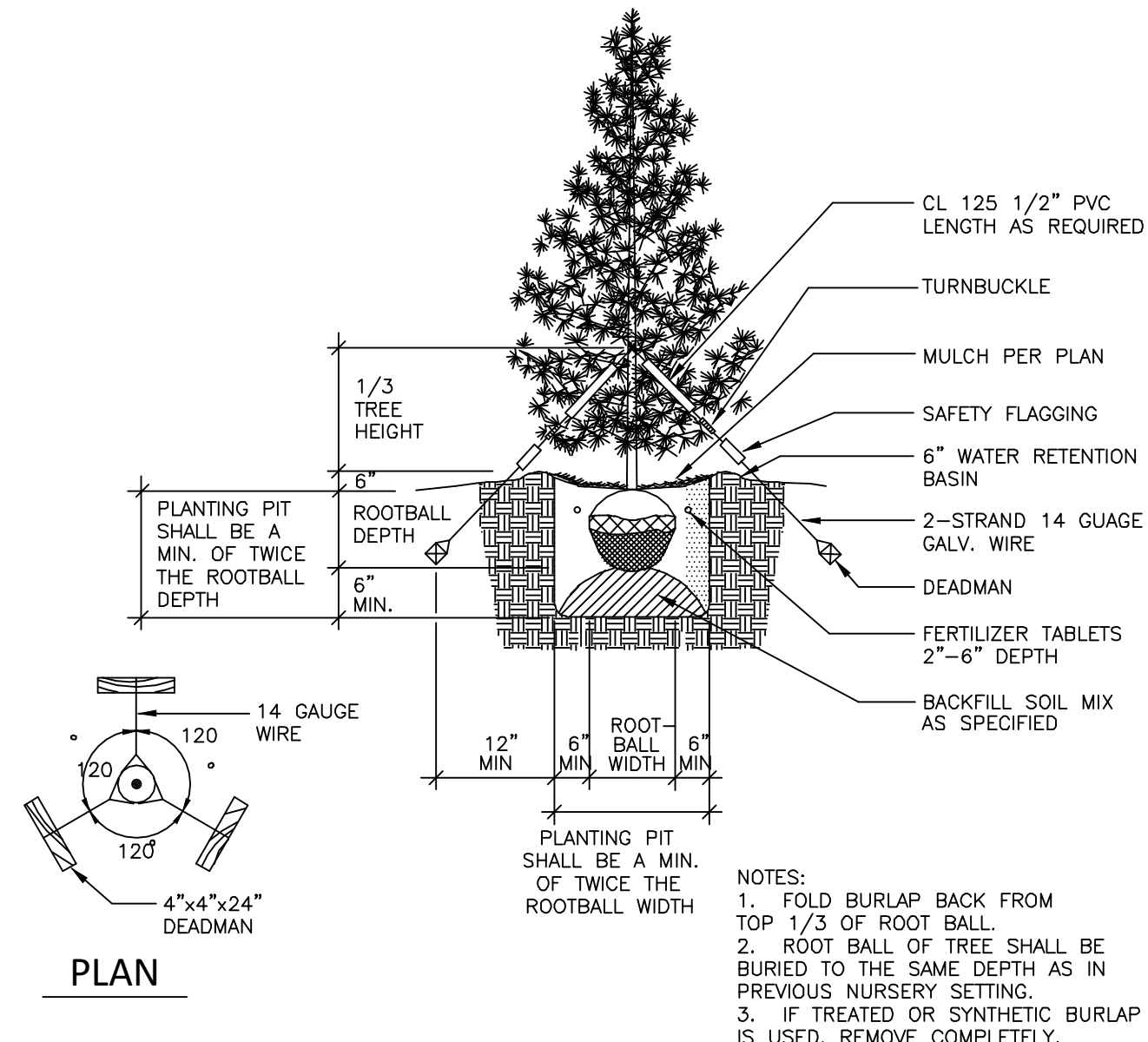
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PROJECT: 16-070
SHEET NO. L2.2

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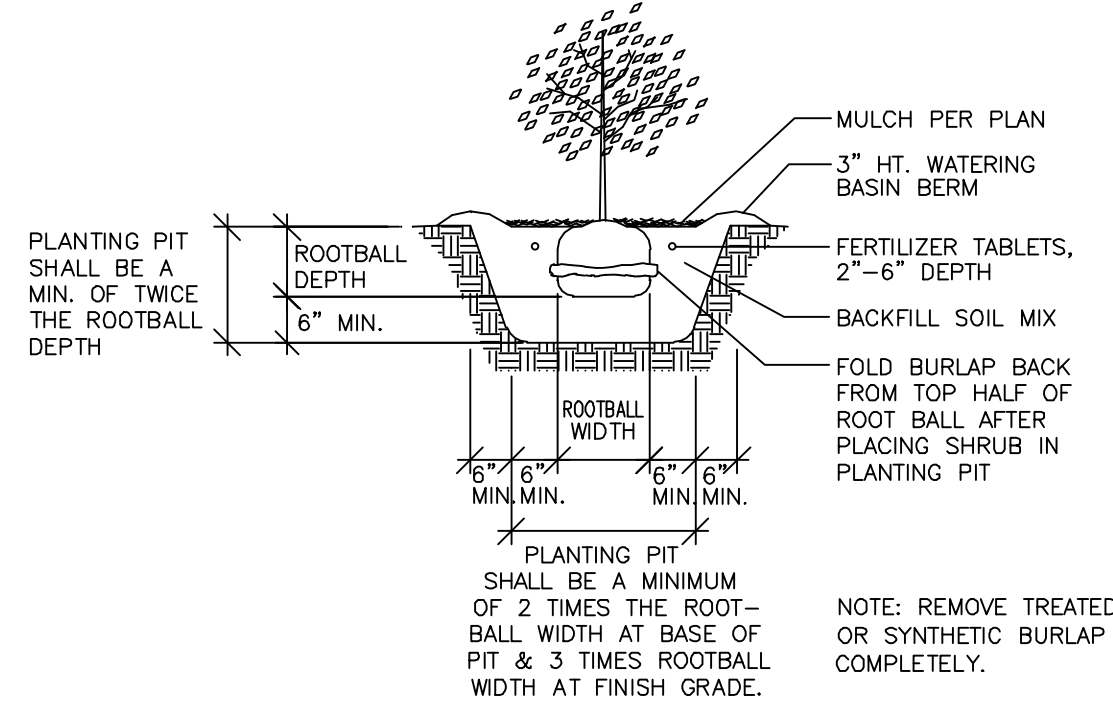
TREE PLANTING AND STAKING DETAIL

NTS



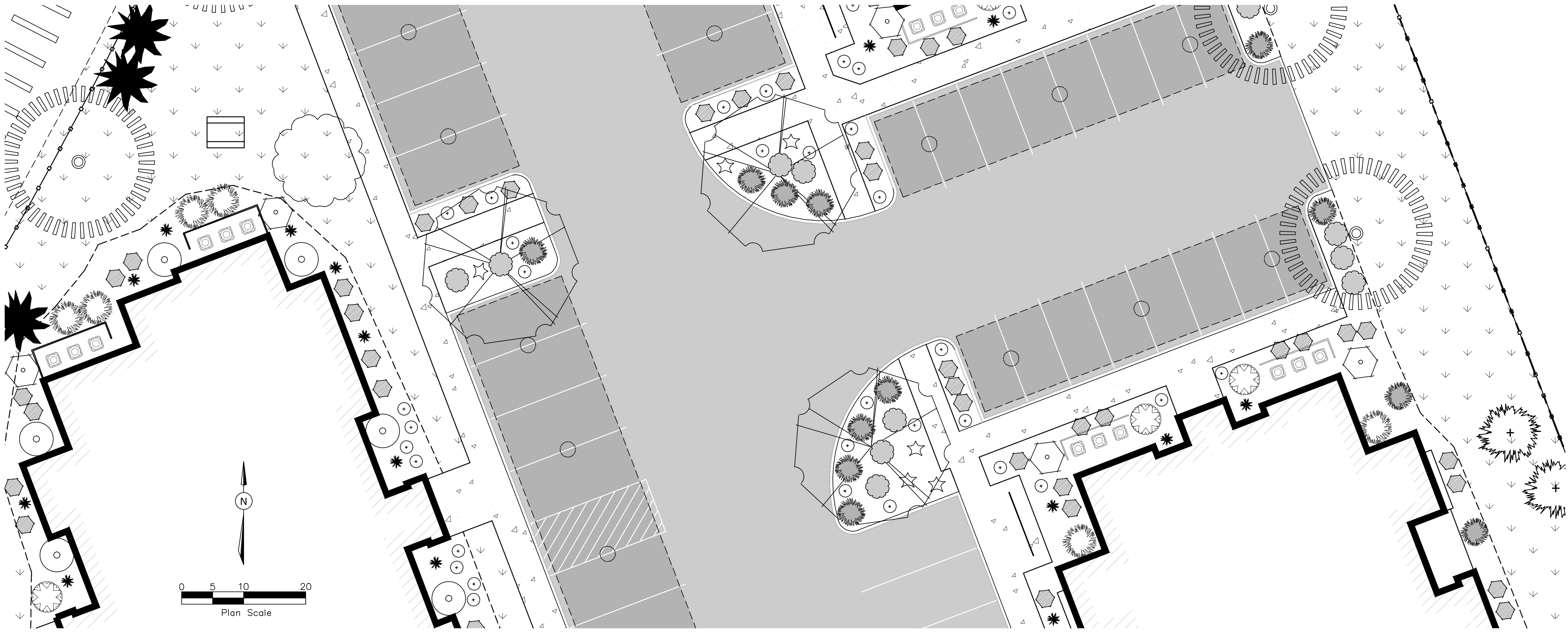
CONIFEROUS TREE PLANTING AND GUYING DETAIL

NTS



SHRUB PLANTING DETAIL

NTS



TYPICAL BUILDING PERIMETER AND PARKING LOT PLANTER ENLARGEMENT

LANDSCAPE LEGEND		
SYMBOL	COMMON NAME	NOTES
	Norwegian Sunset Maple	Class II
	Cimmeron Ash	Class II
	Patmore Ash	Class II
	Greenspire Little Leaf Linden	Class II
	Hoopsii Spruce	Conifer
	Bruns Serbian Spruce	Conifer
	Colorado Blue Spruce	Conifer
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	Dwarf Red Leaf Sand Cherry	large flowering shrub
	Foerster's Feather Reed Grass	Ornamental Grass
	Broadmoor Juniper	evergreen groundcover
	Turf (Sod) Xerilawn, Shadowmaster (both available from The Turf Company, Meridian ID) or approved equal drought-resistant fine fescue	
	Snake River Wheatgrass, Elymus wawawaiensis (seed) or approved equal, native, drought-tolerant perennial grass. These grass areas are intended to be left unmown.	
	Native, perennial seed-mix	
	4' open-view, wrought iron fence	
	6' Solid vinyl fencing	
	install flat-blade shovel edge at all interfaces between turf and planter beds	

SEE SHEET L1.0 FOR COMPLETE PLANT SCHEDULE

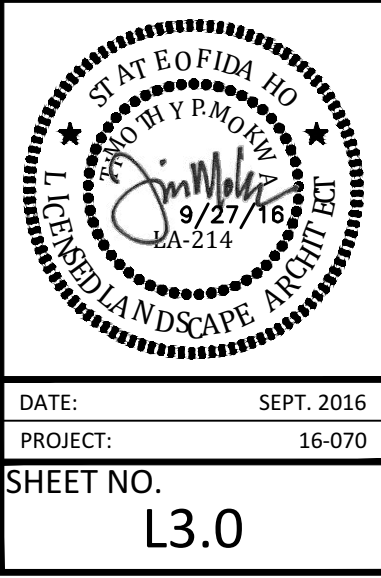


DEPOT VILLAGE APARTMENTS - PUD/REZONE
LANDSCAPE PLAN - PLANTING DETAILS AND ENLARGEMENT
BOISE, IDAHO

REVISIONS		
NO.	ITEM	DATE

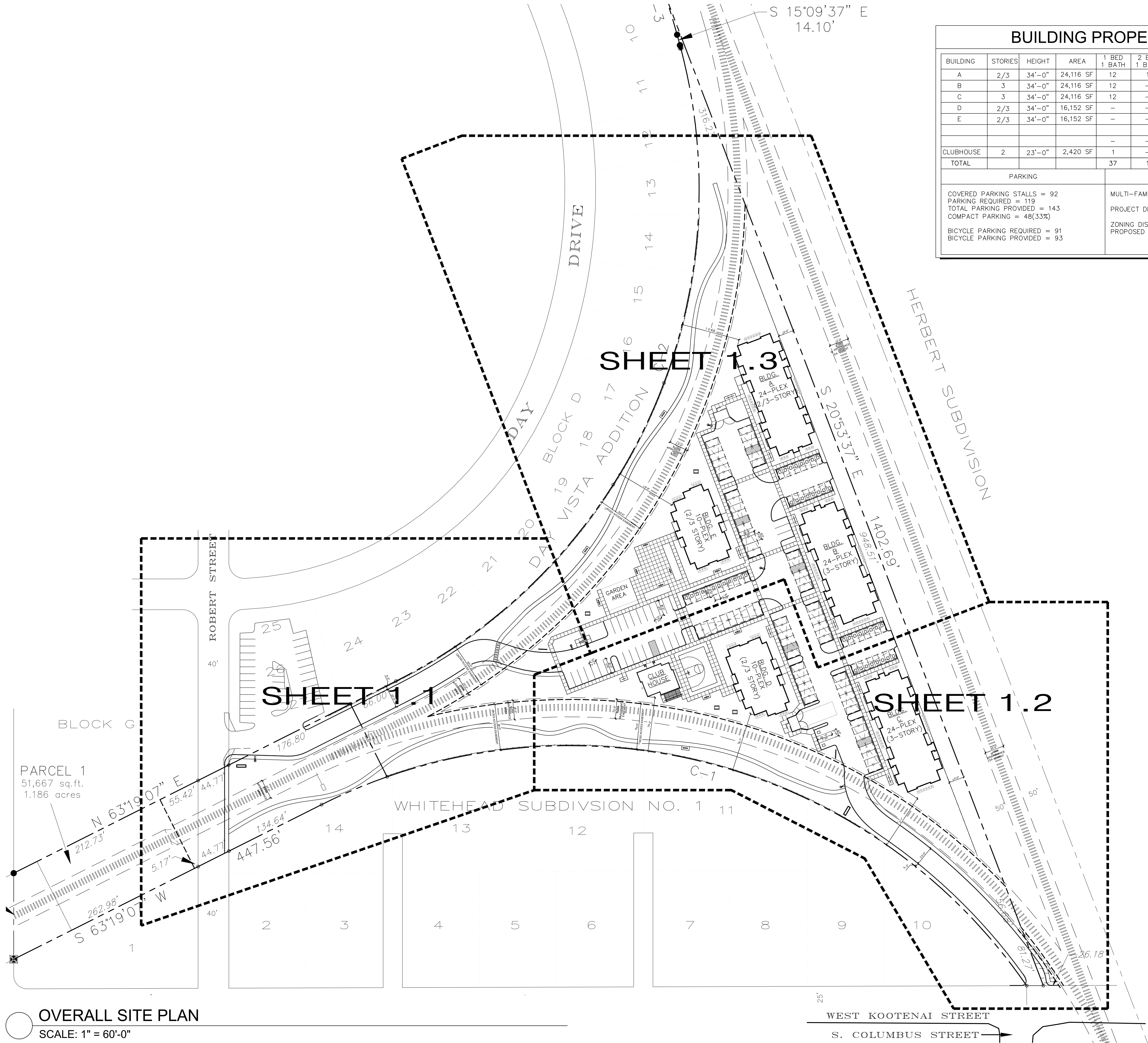
9233 WEST STATE STREET
BOISE, IDAHO 83714
PHONE (208) 639-6939
FAX (208) 639-6930

DATE: SEPT. 2016
PROJECT: 16-070
SHEET NO. L3.0

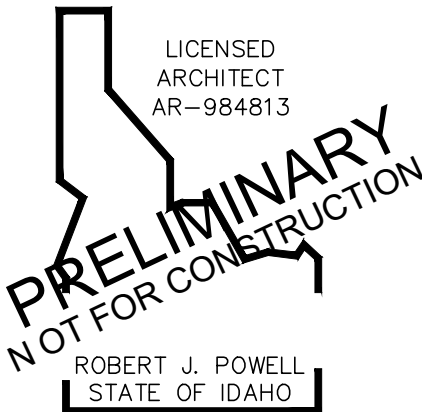
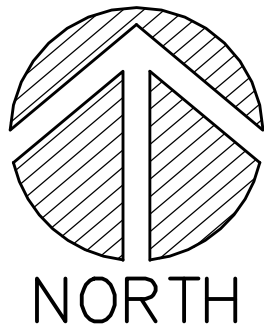


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BUILDING PROPERTIES								
BUILDING	STORIES	HEIGHT	AREA	1 BED 1 BATH	2 BED 1 BATH	2 BED 2 BATH	3 BED 2 BATH	TOTAL UNITS
A	2/3	34'-0"	24,116 SF	12	1	9	-	22
B	3	34'-0"	24,116 SF	12	-	12	-	24
C	3	34'-0"	24,116 SF	12	-	12	-	24
D	2/3	34'-0"	16,152 SF	-	-	2	8	10
E	2/3	34'-0"	16,152 SF	-	-	2	8	10
CLUBHOUSE	2	23'-0"	2,420 SF	1	-	-	-	-
TOTAL				37	1	37	16	91
PARKING				SITE INFORMATION				
COVERED PARKING STALLS = 92 PARKING REQUIRED = 119 TOTAL PARKING PROVIDED = 143 COMPACT PARKING = 48(33%)				MULTI-FAMILY PROJECT AREA = 7.39 ACRES PROJECT DENSITY = 12.31 UNITS/ACRE ZONING DISTRICT = CURRENT (R-1C) PROPOSED = R-2				
BICYCLE PARKING REQUIRED = 91 BICYCLE PARKING PROVIDED = 93								



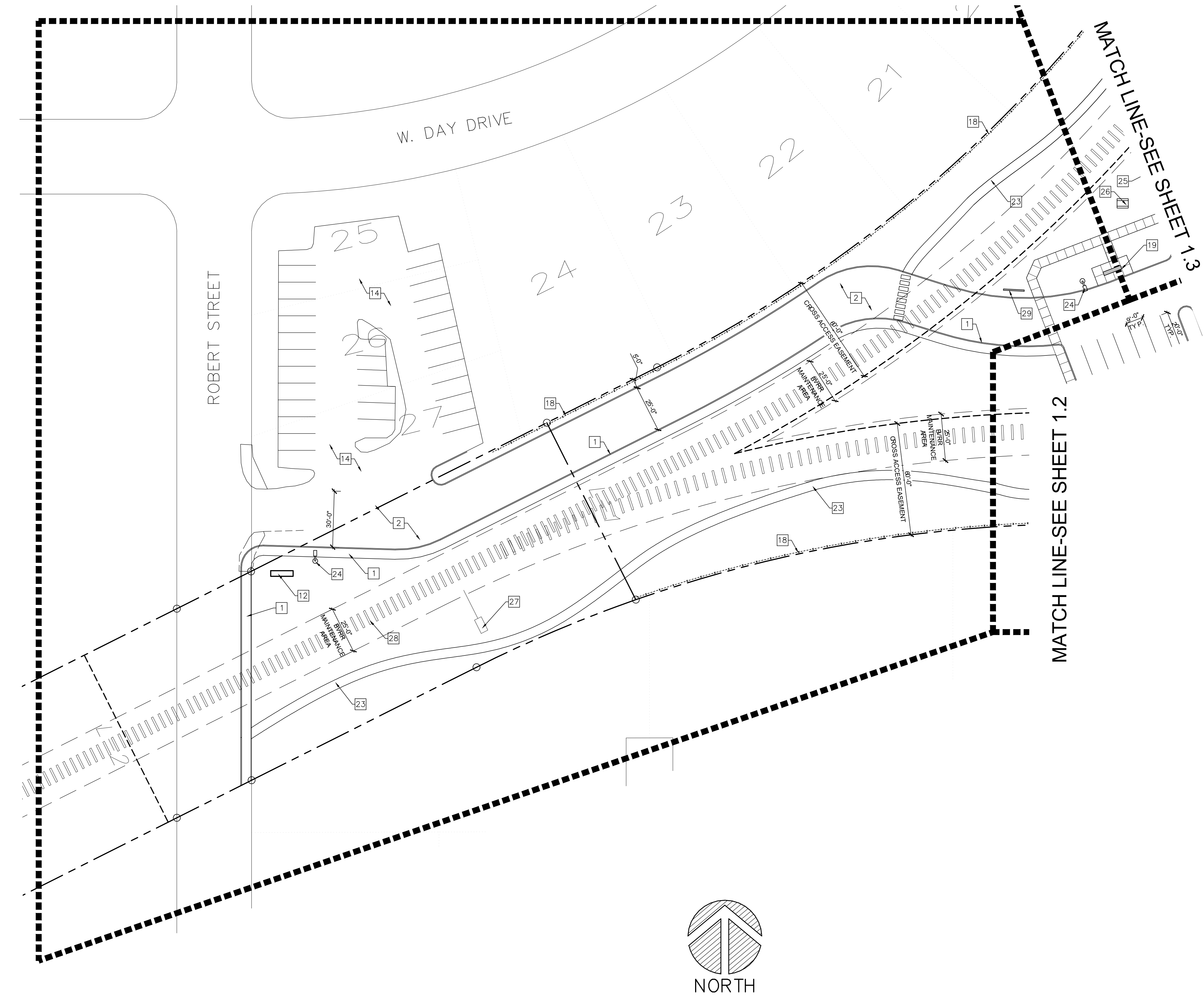
STATION VILLAGE
APARTMENTS

2350 W. KOOTENAI ST.
BOISE, ID 83705

REVISIONS

ARCH. INDEX

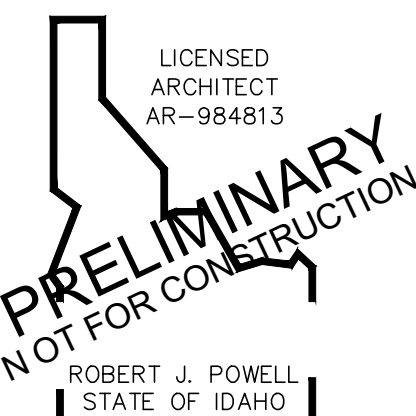
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 SITE PLAN
SCALE: 1" = 30'-0"

KEY NOTES	
1	4" CONCRETE SIDEWALK OVER 4" COMPACTED GRAVEL W/ SCORED CONTROL JOINTS PER PLAN @ 5'-0" MAX O.C. - SEE CIVIL DRAWINGS.
2	ASPHALT PAVING - SEE CIVIL
3	NEW CURB - SEE CIVIL
4	PROPOSED ELECTRICAL TRANSFORMER - SEE ELEC.
5	BICYCLE RACK -SEE DETAIL ON SHEET A1.4
6	EXISTING FIRE HYDRANT
7	EXISTING SIDEWALK
8	NEW TRASH ENCLOSURE - SEE DETAIL 1/A1.0
9	PROVIDE HANDICAP PARKING SIGN - SEE CIVIL
10	AIR CONDENSER UNITS -SCREENED BY 4' VINYL FENCE
11	PRIVATE PATIO AT GRADE & PRIVATE BALCONY ABOVE
12	PROPOSED MONUMENT SIGN
13	PRE-FINISHED STEEL CARPORT ABOVE
14	EXISTING ASPHALT PARKING
15	NOT USED
16	SAW-CUT CONCRETE PLAZA AREA
17	RETAINING WALL -SEE CIVIL & LANDSCAPE
18	NEW 6' VINYL FENCE
19	COMMUNITY MAILBOX LOCATION ON CONC. SLAB
20	CONCRETE PEDESTRIAN SIDEWALK
21	NEW FIRE HYDRANT - SEE CIVIL
22	OVERHEAD WOOD TRELLIS -SEE CLUBHOUSE ELEV.
23	GRAVEL WALKING PATH
24	LIGHT POLE -SEE ELECTRICAL PLANS
25	SURFACE MOUNT - 6' PARK BENCH WITH METAL FRAME
26	6" PICNIC TABLE WITH PRESSURE TREATED WOOD AND POWDER COATED PRE-GALVANIZED STRUCTURAL STEEL TUBING FRAME
27	EXISTING RAILROAD SWITCH
28	EXISTING RAILROAD TRACKS
29	MAP & DIRECTORY OF COMPLEX

SEP. 27, 2016



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ROBERT J. POWELL
STATE OF IDAHO



DAVE EVANS
CONSTRUCTION
7761 W. Riverside Dr.
Boise, ID 83714
Office 853-1203 Fax 853-1220
www.daveevansconstruction.com

STATION VILLAGE
APARTMENTS

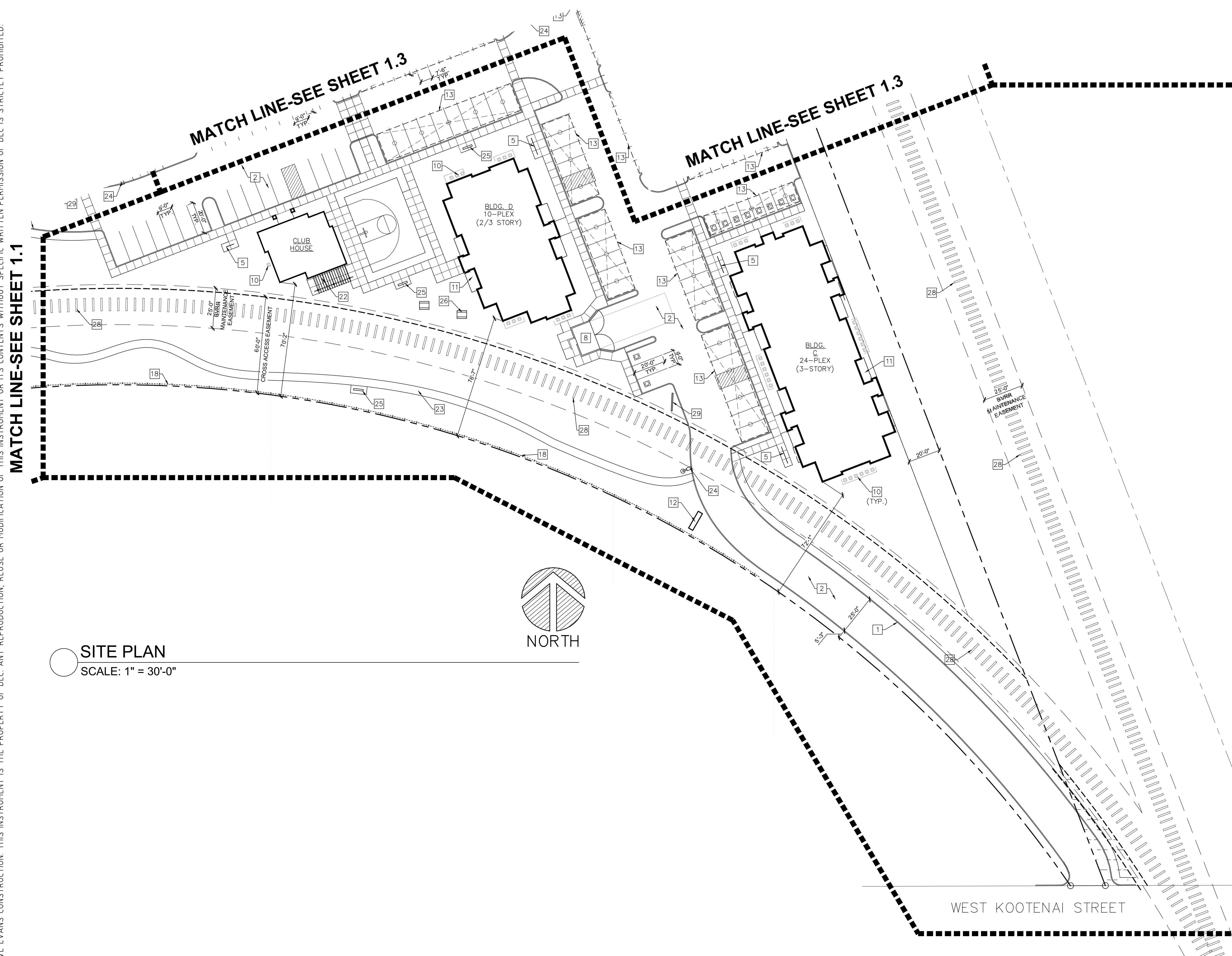
2350 W. KOOTENAI ST.
BOISE, ID 83705

REVISIONS

ARCH. INDEX

A1.1

SITE PLAN



KEY NOTES

- 1 4" CONCRETE SIDEWALK OVER 4" COMPACTED GRAVEL W/ SCORED CONTROL JOINTS PER PLAN @ 5'-0" MAX O.C. - SEE CIVIL DRAWINGS.
- 2 ASPHALT PAVING - SEE CIVIL
- 3 NEW CURB - SEE CIVIL
- 4 PROPOSED ELECTRICAL TRANSFORMER - SEE ELEC.
- 5 BICYCLE RACK -SEE DETAIL ON SHEET A1.4
- 6 EXISTING FIRE HYDRANT
- 7 EXISTING SIDEWALK
- 8 NEW TRASH ENCLOSURE - SEE DETAIL 1/A1.0
- 9 PROVIDE HANDICAP PARKING SIGN - SEE CIVIL
- 10 AIR CONDENSER UNITS -SCREENED BY 4' VINYL FENCE
- 11 PRIVATE PATIO AT GRADE & PRIVATE BALCONY ABOVE
- 12 PROPOSED MONUMENT SIGN
- 13 PRE-FINISHED STEEL CARPORT ABOVE
- 14 EXISTING ASPHALT PARKING
- 15 NOT USED
- 16 SAW-CUT CONCRETE PLAZA AREA
- 17 RETAINING WALL -SEE CIVIL & LANDSCAPE
- 18 NEW 6' VINYL FENCE
- 19 COMMUNITY MAILBOX LOCATION ON CONC. SLAB
- 20 CONCRETE PEDESTRIAN SIDEWALK
- 21 NEW FIRE HYDRANT - SEE CIVIL
- 22 OVERHEAD WOOD TRELLIS -SEE CLUBHOUSE ELEV.
- 23 GRAVEL WALKING PATH
- 24 LIGHT POLE -SEE ELECTRICAL PLANS
- 25 SURFACE MOUNT - 6' PARK BENCH WITH METAL FRAME
- 26 6' PICNIC TABLE WITH PRESSURE TREATED WOOD AND POWDER COATED PRE-GALVANIZED STRUCTURAL STEEL TUBING FRAME
- 27 EXISTING RAILROAD SWITCH
- 28 EXISTING RAILROAD TRACKS
- 29 MAP & DIRECTORY OF COMPLEX

STATION VILLAGE
APARTMENTS

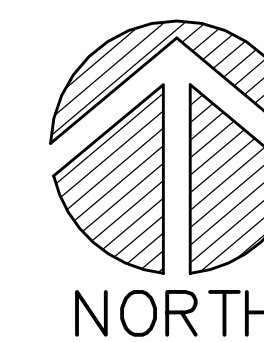
2350 W. KOOTENAI ST.
BOISE, ID 83705

REVISIONS

ARCH. INDEX

A1.2

SITE PLAN



- 1 4" CONCRETE SIDEWALK OVER 4" COMPACTED GRAVEL
W/ SCORED CONTROL JOINTS PER PLAN @ 5'-0" MAX
O.C. - SEE CIVIL DRAWINGS.
- 2 ASPHALT PAVING - SEE CIVIL
- 3 NEW CURB - SEE CIVIL
- 4 PROPOSED ELECTRICAL TRANSFORMER - SEE ELEC.
- 5 BICYCLE RACK -SEE DETAIL ON SHEET A1.4
- 6 EXISTING FIRE HYDRANT
- 7 EXISTING SIDEWALK
- 8 NEW TRASH ENCLOSURE - SEE DETAIL 1/A1.0
- 9 PROVIDE HANDICAP PARKING SIGN - SEE CIVIL
- 10 AIR CONDENSER UNITS -SCREENED BY 4' VINYL FENCE
- 11 PRIVATE PATIO AT GRADE & PRIVATE BALCONY ABOVE
- 12 PROPOSED MONUMENT SIGN
- 13 PRE-FINISHED STEEL CARPORT ABOVE
- 14 EXISTING ASPHALT PARKING
- 15 NOT USED
- 16 SAW-CUT CONCRETE PLAZA AREA
- 17 RETAINING WALL -SEE CIVIL & LANDSCAPE
- 18 NEW 6' VINYL FENCE
- 19 COMMUNITY MAILBOX LOCATION ON CONC. SLAB
- 20 CONCRETE PEDESTRIAN SIDEWALK
- 21 NEW FIRE HYDRANT - SEE CIVIL
- 22 OVERHEAD WOOD TRELLIS -SEE CLUBHOUSE ELEVS.
- 23 GRAVEL WALKING PATH
- 24 LIGHT POLE -SEE ELECTRICAL PLANS
- 25 SURFACE MOUNT - 6' PARK BENCH WITH METAL FRAME
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POWDER COATED PRE-GALVANIZED STRUCTURAL STEEL
TUBING FRAME
- 27 EXISTING RAILROAD SWITCH
- 28 EXISTING RAILROAD TRACKS
- 29 MAP & DIRECTORY OF COMPLEX

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STATE OF IDAHO

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Boise, ID 83714

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www.devansconstruction.com

STATION VILLAGE APARTMENTS

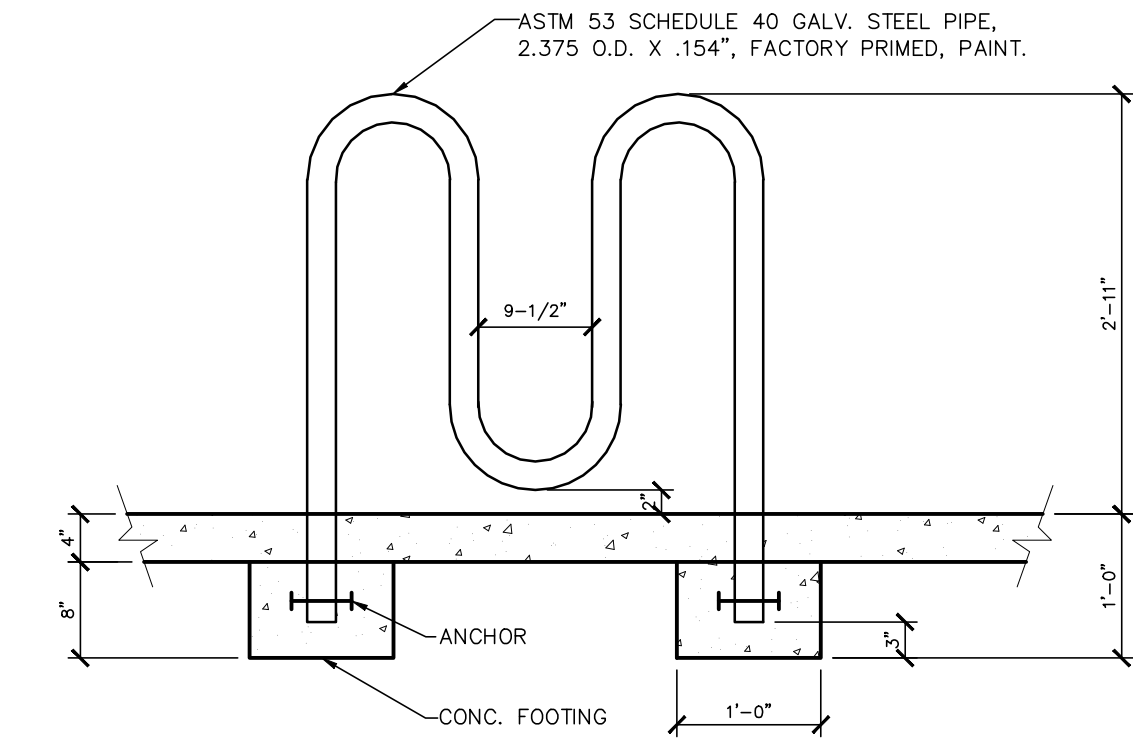
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BOISE, ID 83705

REVISIONS

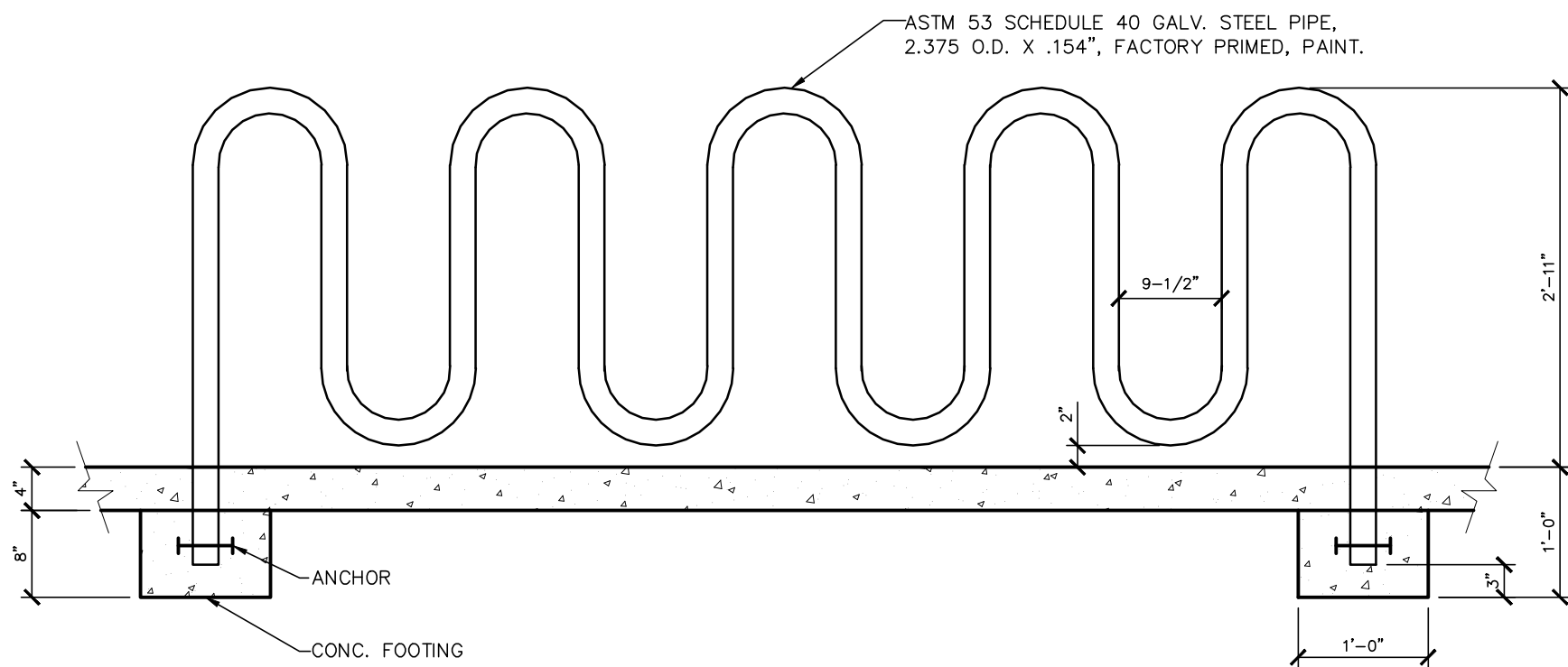
ARCH. INDEX

A1.3

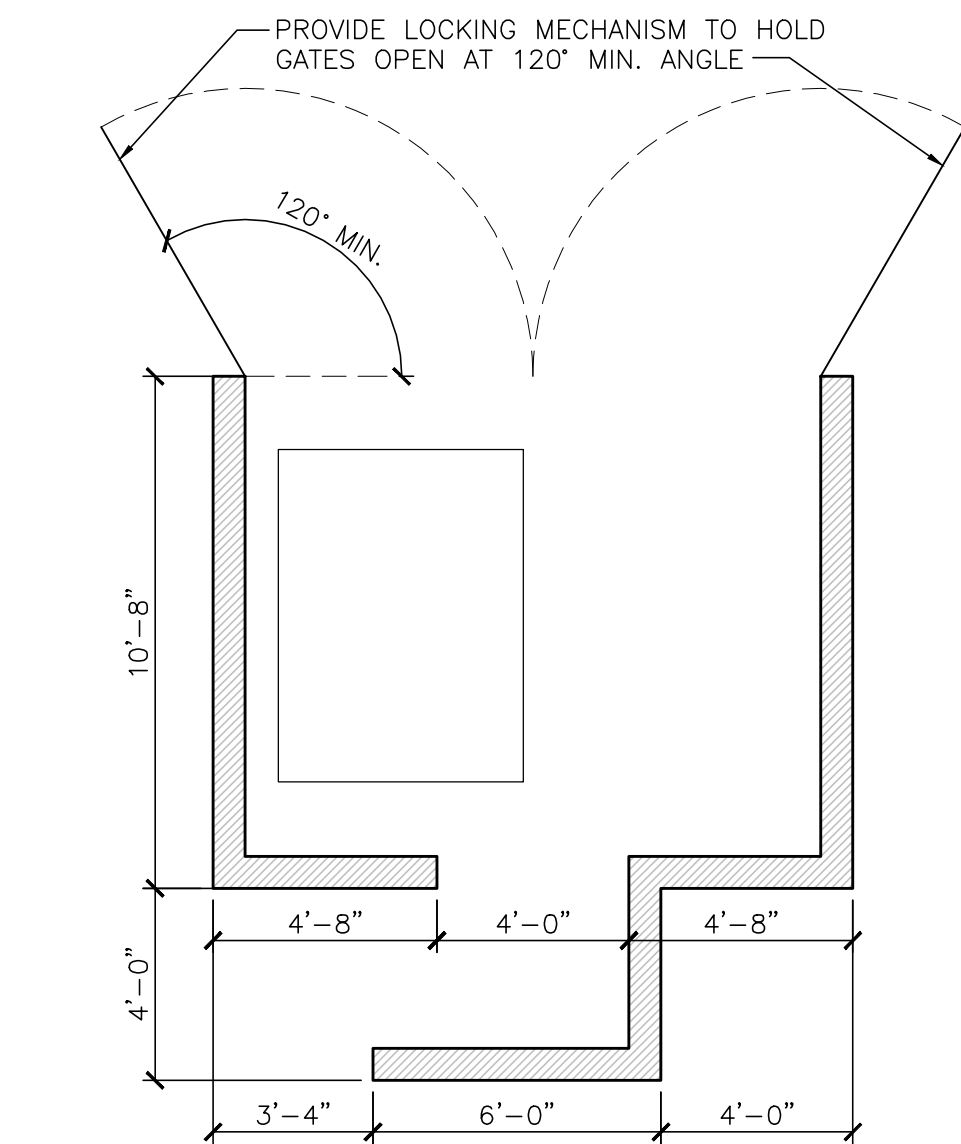
SITE PLAN



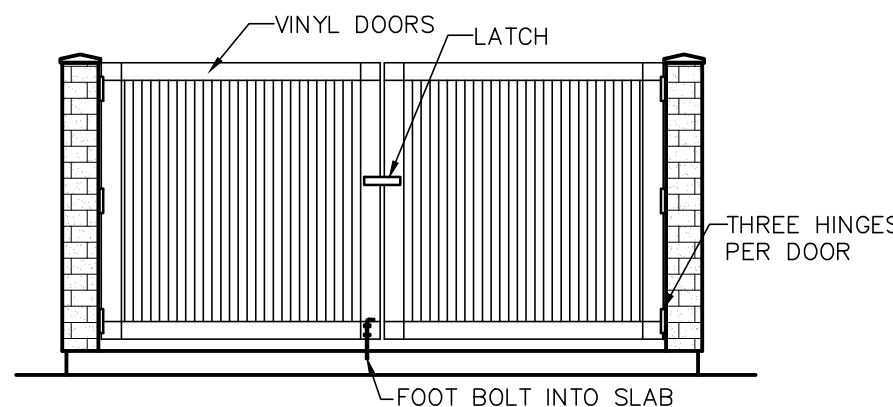
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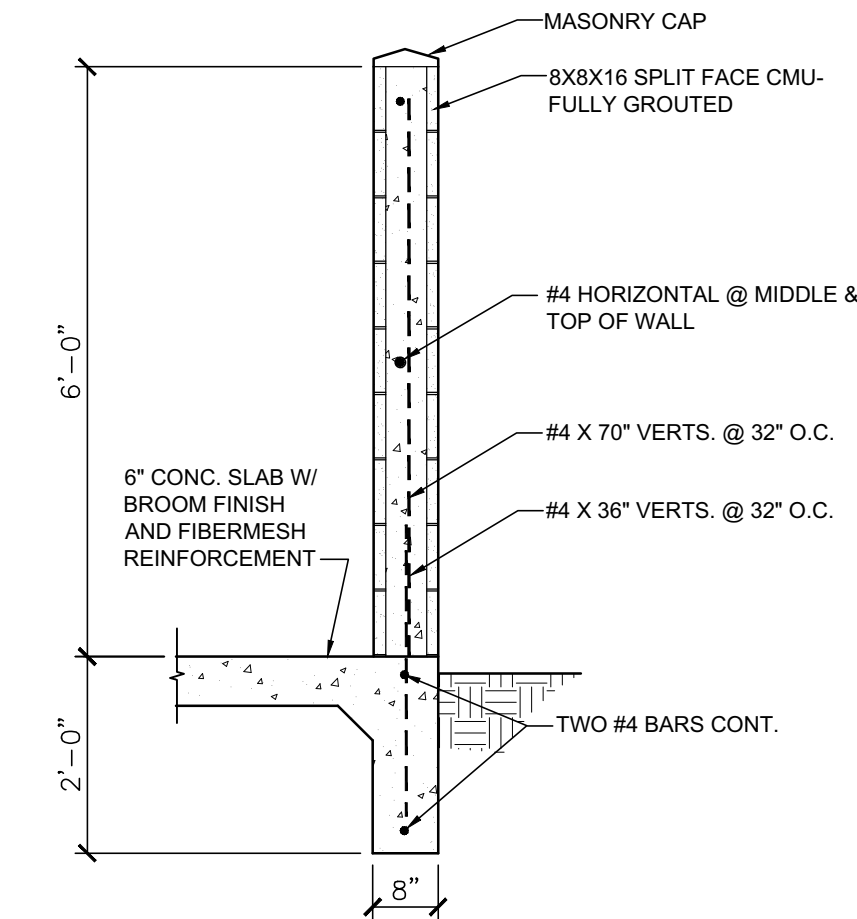
3 11 BIKE RACK DETAIL
SCALE: N.T.S.



PLAN VIEW

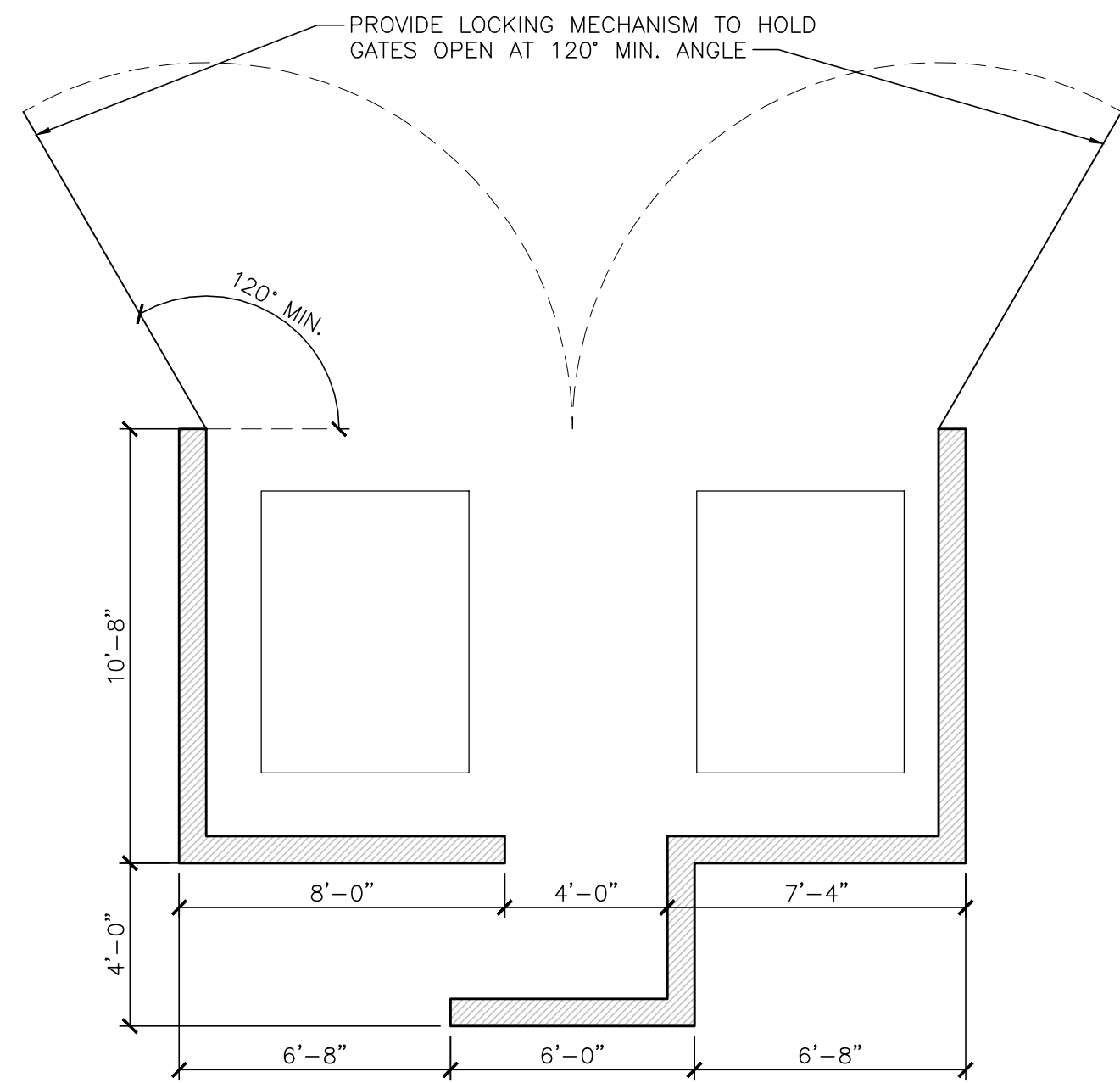


FRONT ELEVATION

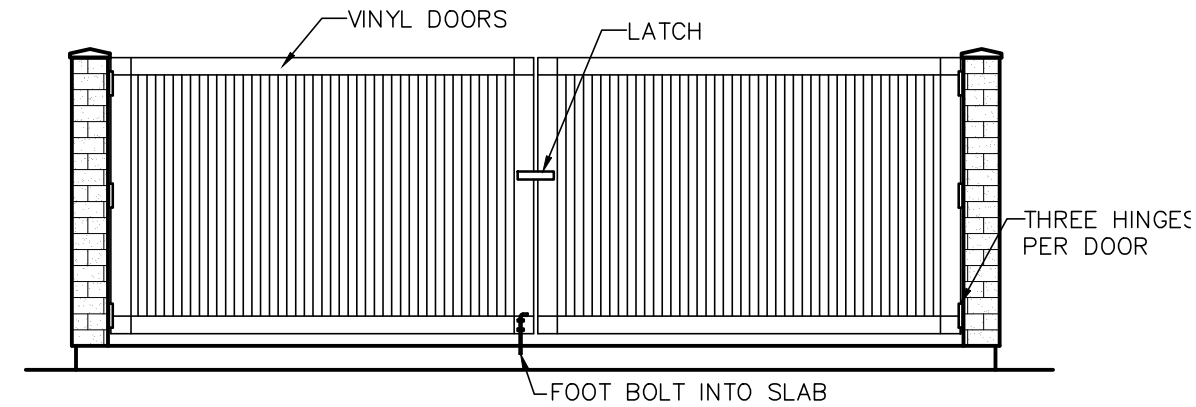


WALL SECTION

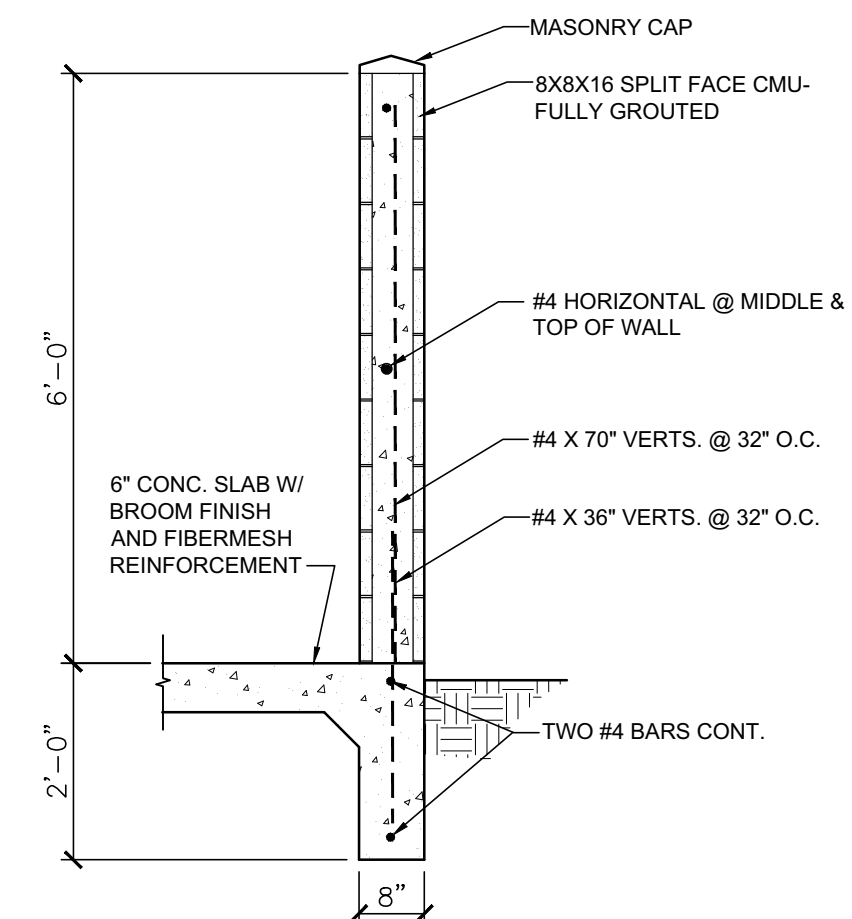
2 SINGLE DUMPSTER ENCLOSURE
SCALE: N.T.S.



PLAN VIEW

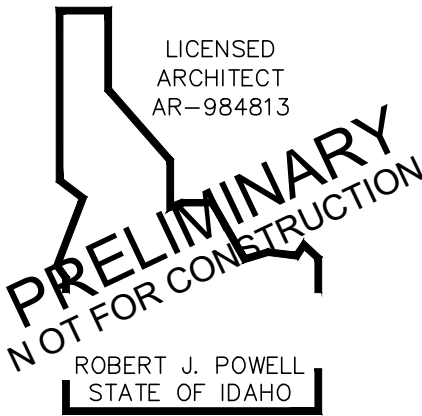


FRONT ELEVATION



WALL SECTION

1 DOUBLE DUMPSTER ENCLOSURE
SCALE: N.T.S.



STATION VILLAGE
APARTMENTS

2350 W. KOOTENAI ST.
BOISE, ID 83705

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FRONT ELEVATION
SCALE: 1/8" = 1'-0"



SIDE ELEVATION
SCALE: 1/8" = 1'-0"



SIDE ELEVATION
SCALE: 1/8" = 1'-0"



REAR ELEVATION
SCALE: 1/8" = 1'-0"

KEYNOTES

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- 13 INSUL. STEEL DOOR - COLOR:
- 14 WALL MOUNTED LIGHT FIXTURE - SEE ELEC.
- 15 ONE HOUR RATED DOOR
- 16 HINGED PATIO DOOR
- 17 GAS METER LOCATION
- 18 POWER METER LOCATION
- 19 10" ADDRESS NUMBERS
- 20 NOT USED

SEP. 27, 2016

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STATION VILLAGE
APARTMENTS
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A4.0
ELEVATIONS
10-PLEX

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SIDE ELEVATION
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BOISE, ID 83705

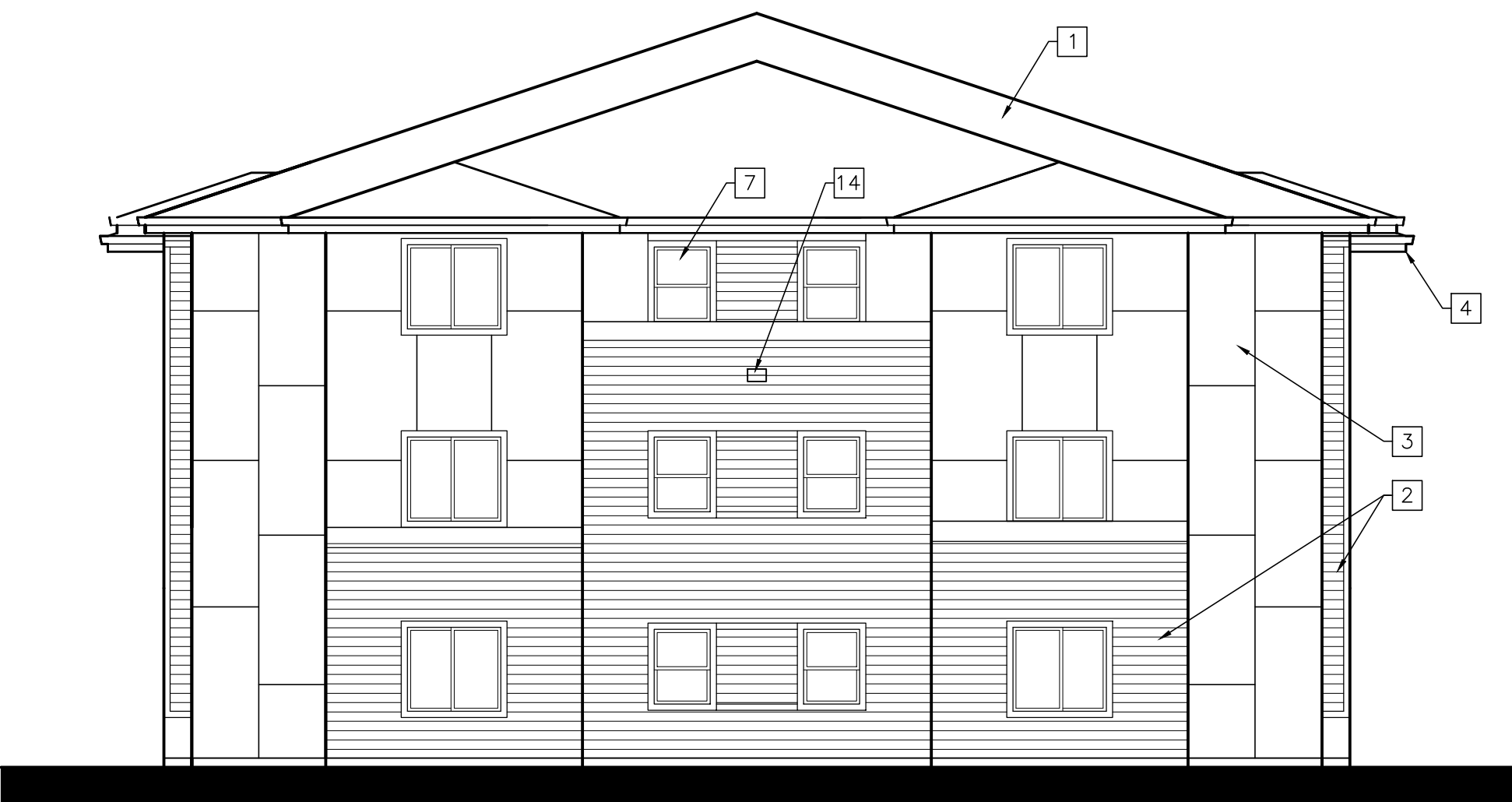
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FRONT ELEVATION
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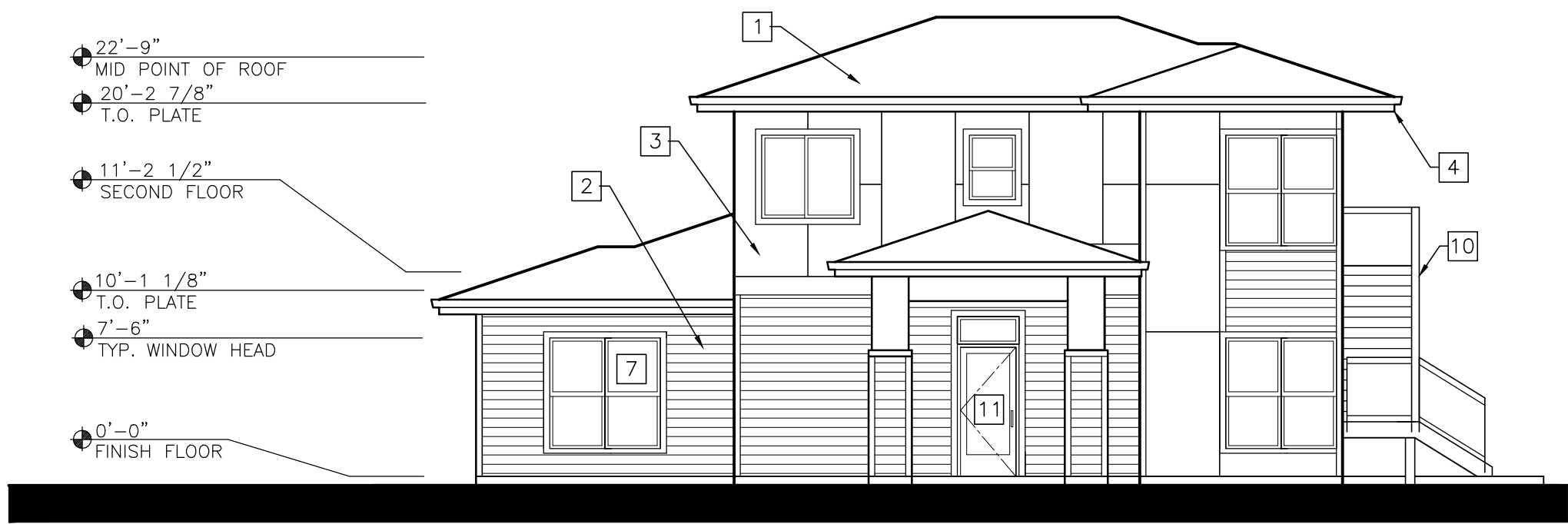
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2350 W. KOOTENAI ST.
BOISE, ID 83705

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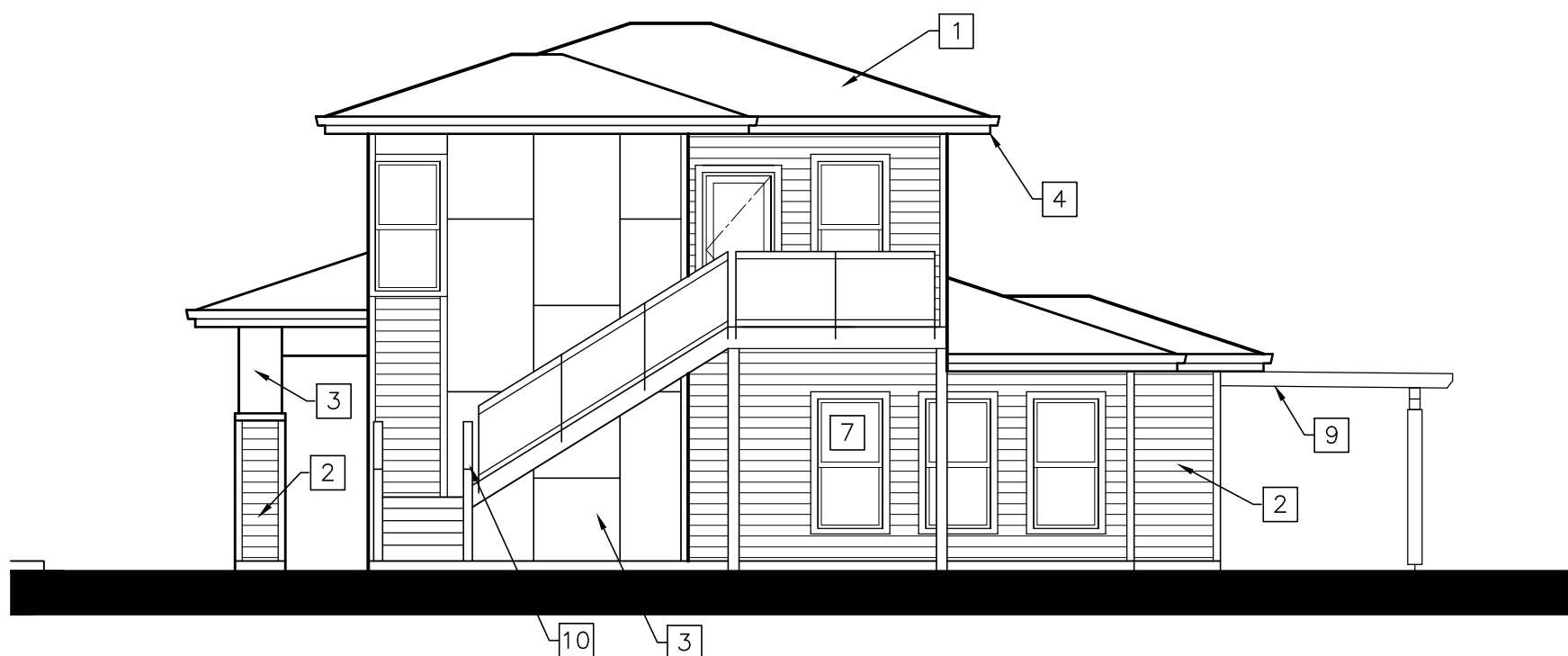
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A4.2
ELEVATIONS
24-PLEX

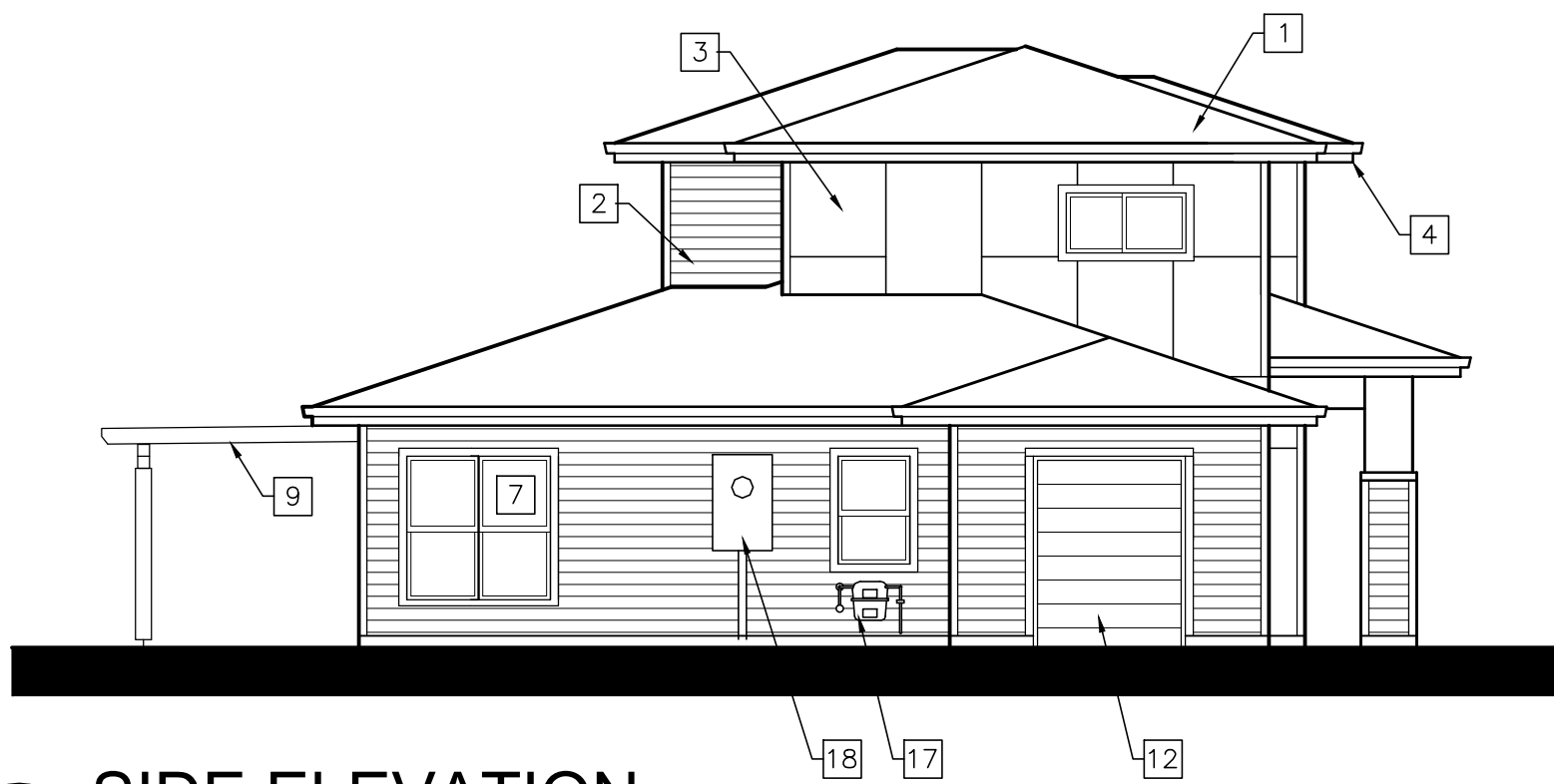
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FRONT ELEVATION
SCALE: 1/8" = 1'-0"



SIDE ELEVATION
SCALE: 1/8" = 1'-0"



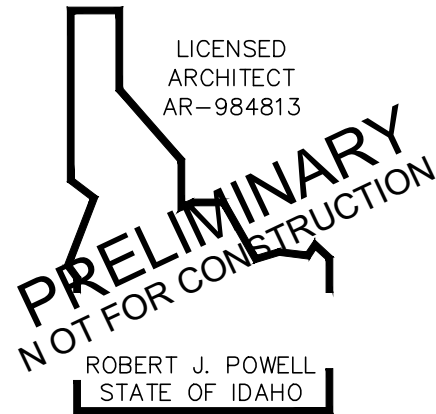
SIDE ELEVATION
SCALE: 1/8" = 1'-0"



REAR ELEVATION
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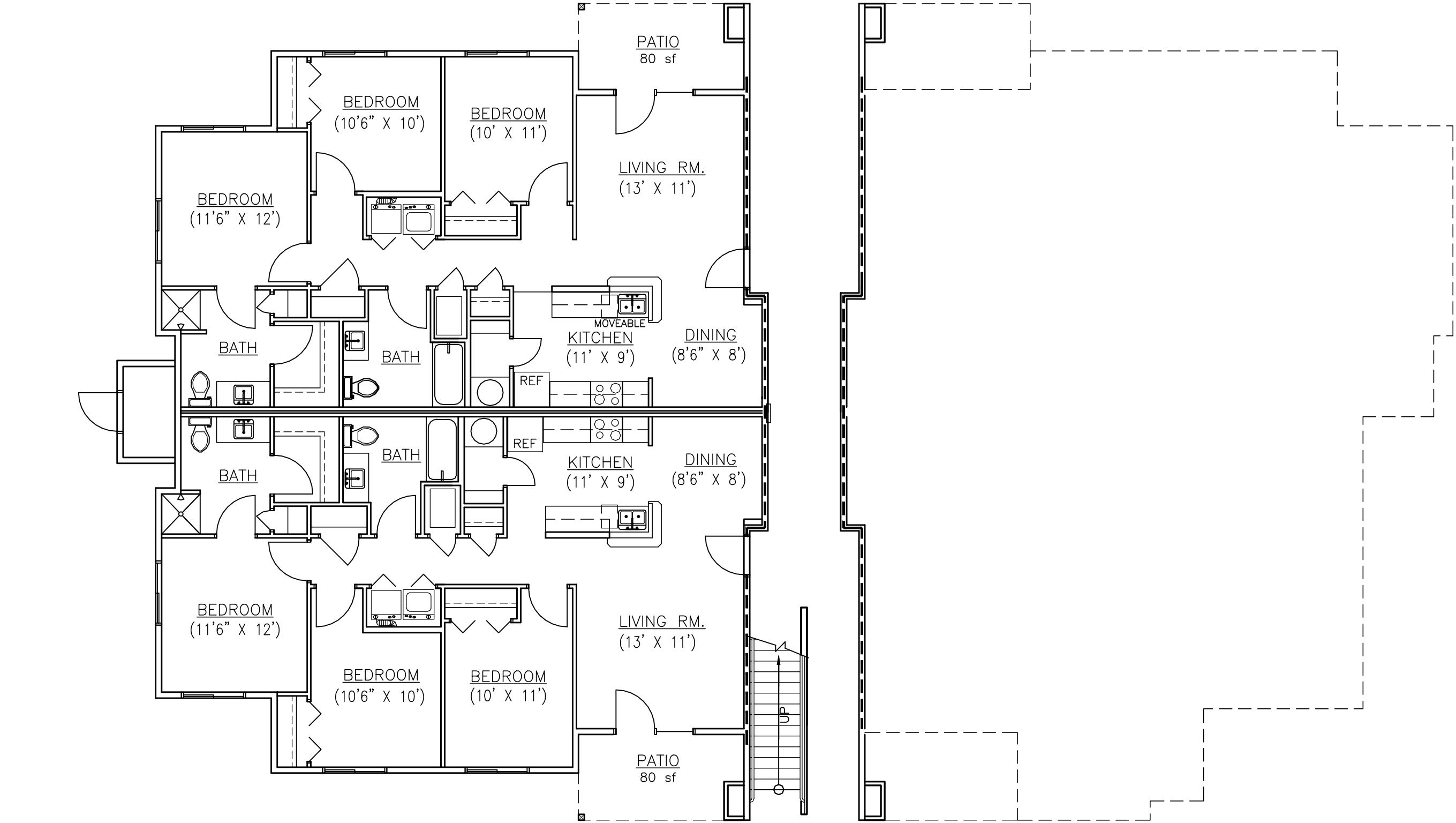
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BOISE, ID 83705

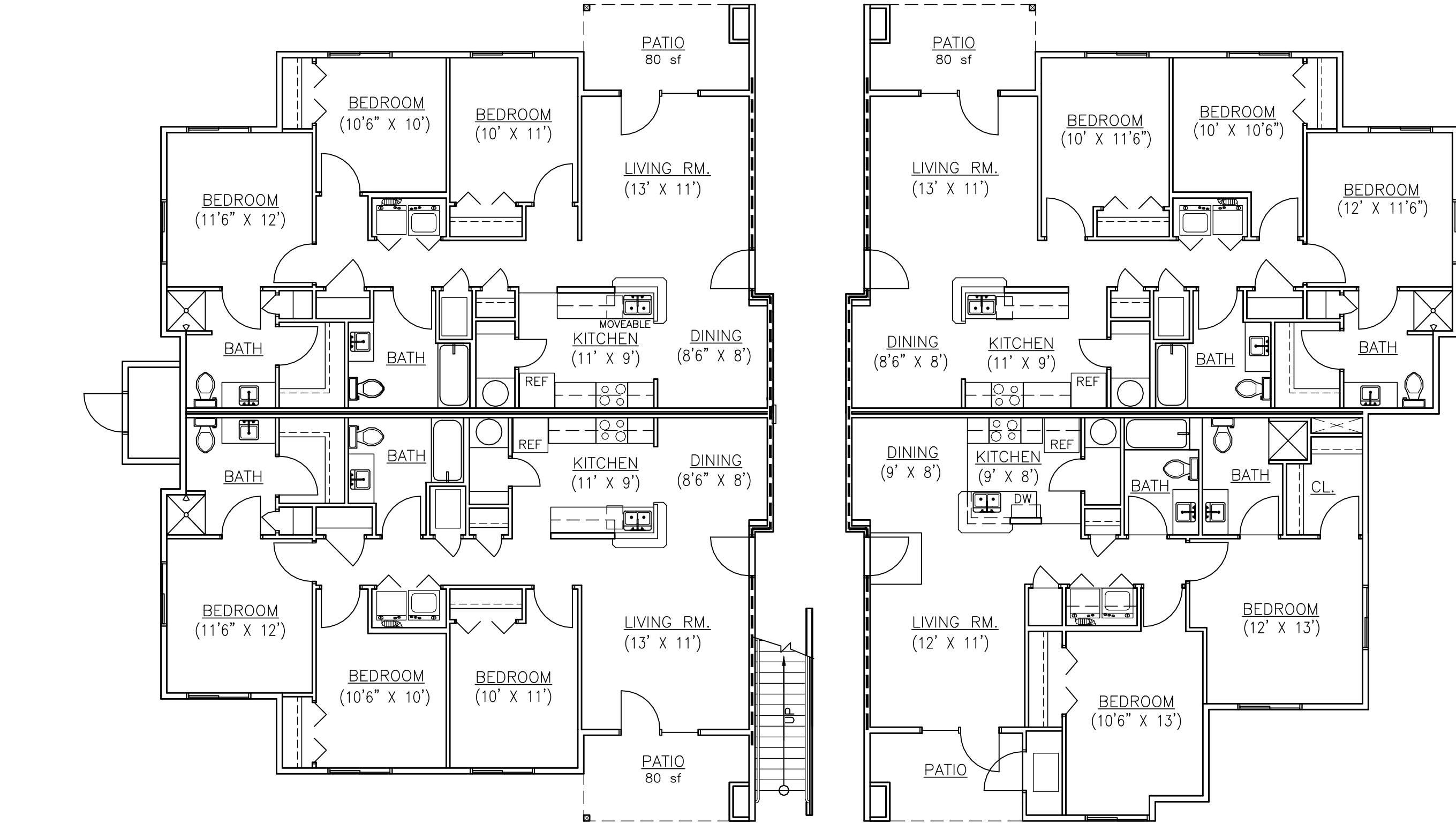
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A4.3
ELEVATIONS
CLUBHOUSE



THIRD FLOOR PLAN - 10-PLEX
SCALE: 1/8" = 1'-0"



FIRST / SECOND FLOOR PLAN - 10-PLEX
SCALE: 1/8" = 1'-0"

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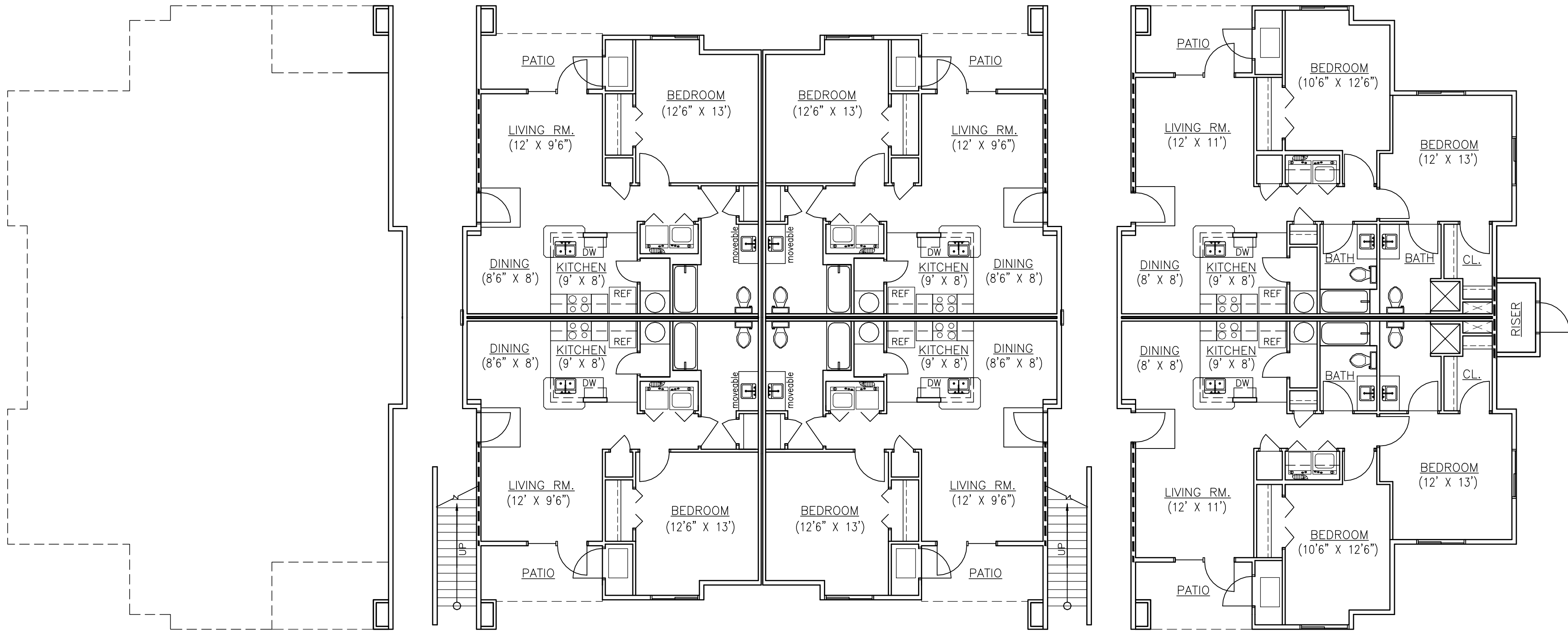
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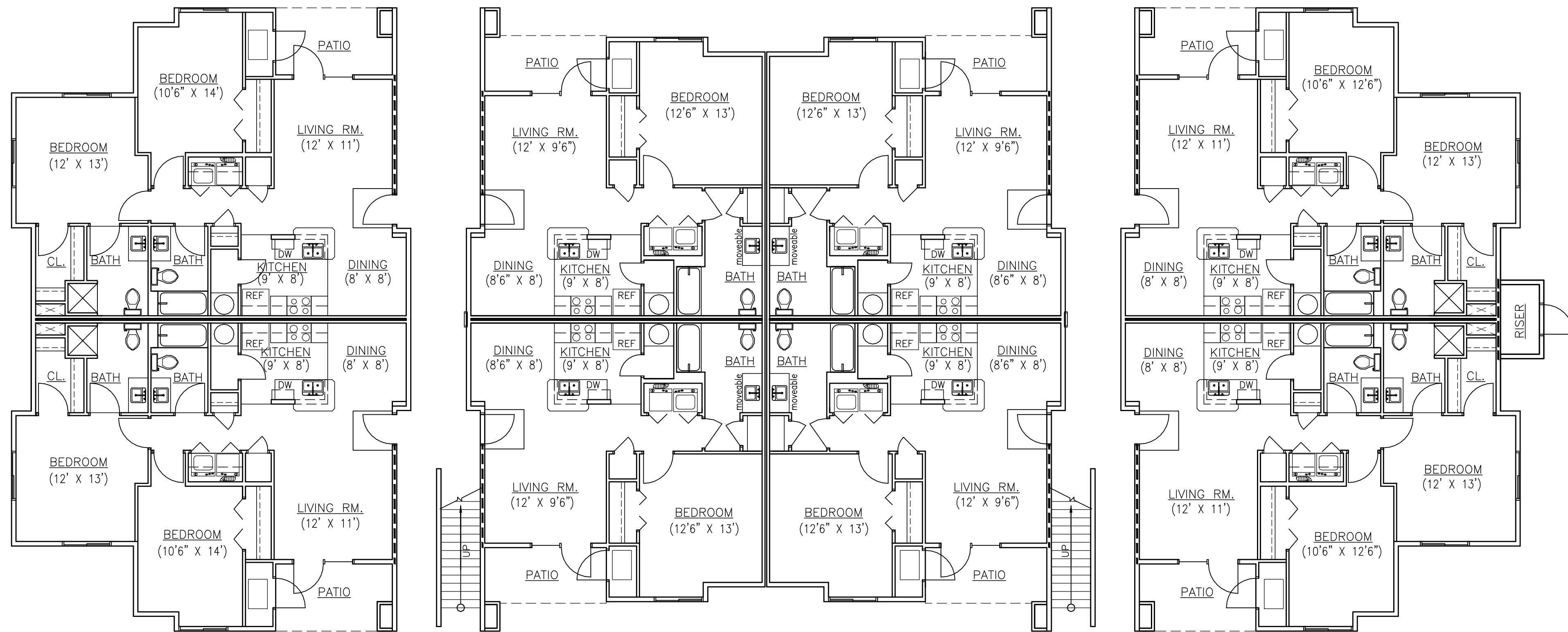
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A2.1
FLOOR PLANS
10-PLEX



○ THIRD FLOOR PLAN - 22-PLEX
SCALE: 1/8" = 1'-0"



○ FIRST / SECOND FLOOR PLAN - 22-PLEX
SCALE: 1/8" = 1'-0"

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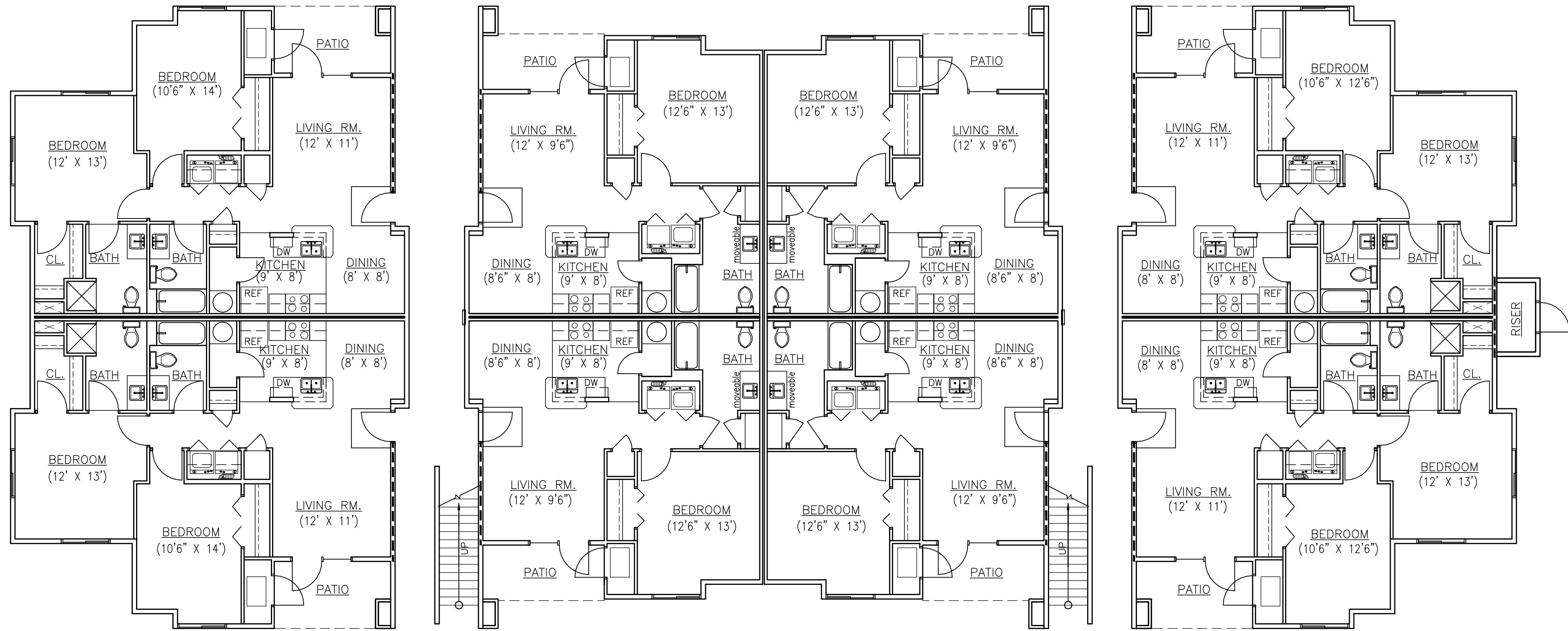
**STATION VILLAGE
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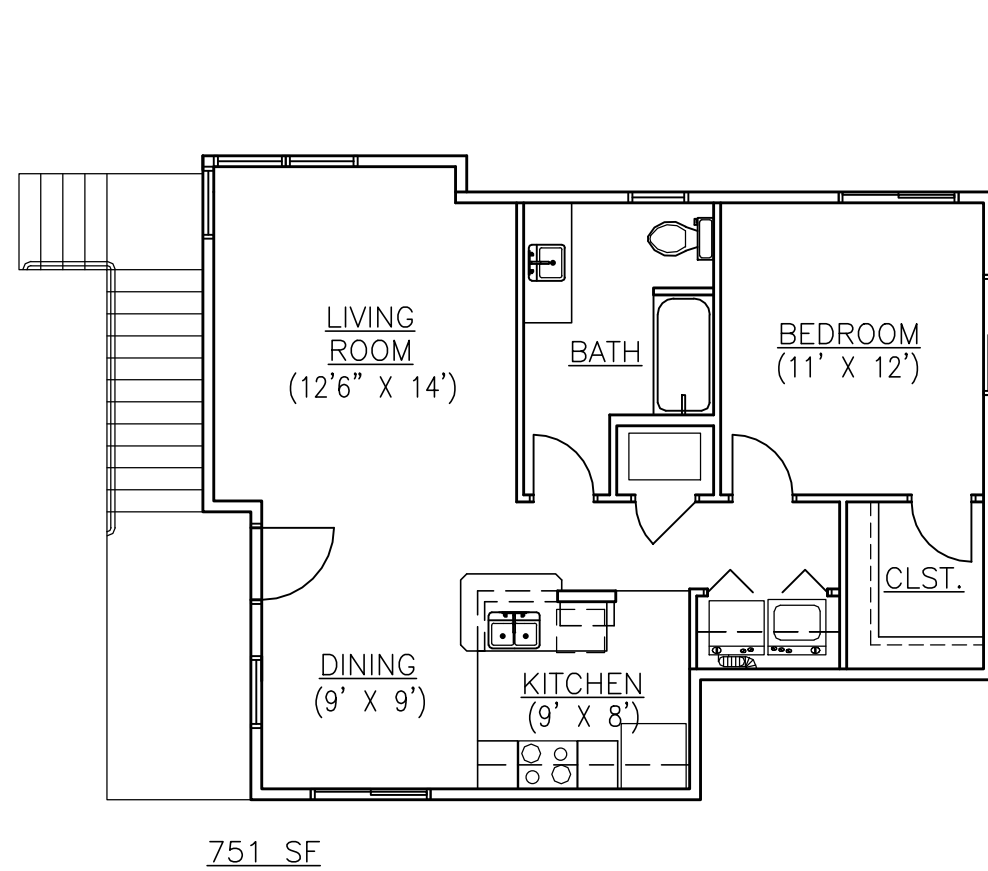
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A2.2
FLOOR PLANS
22-PLEX

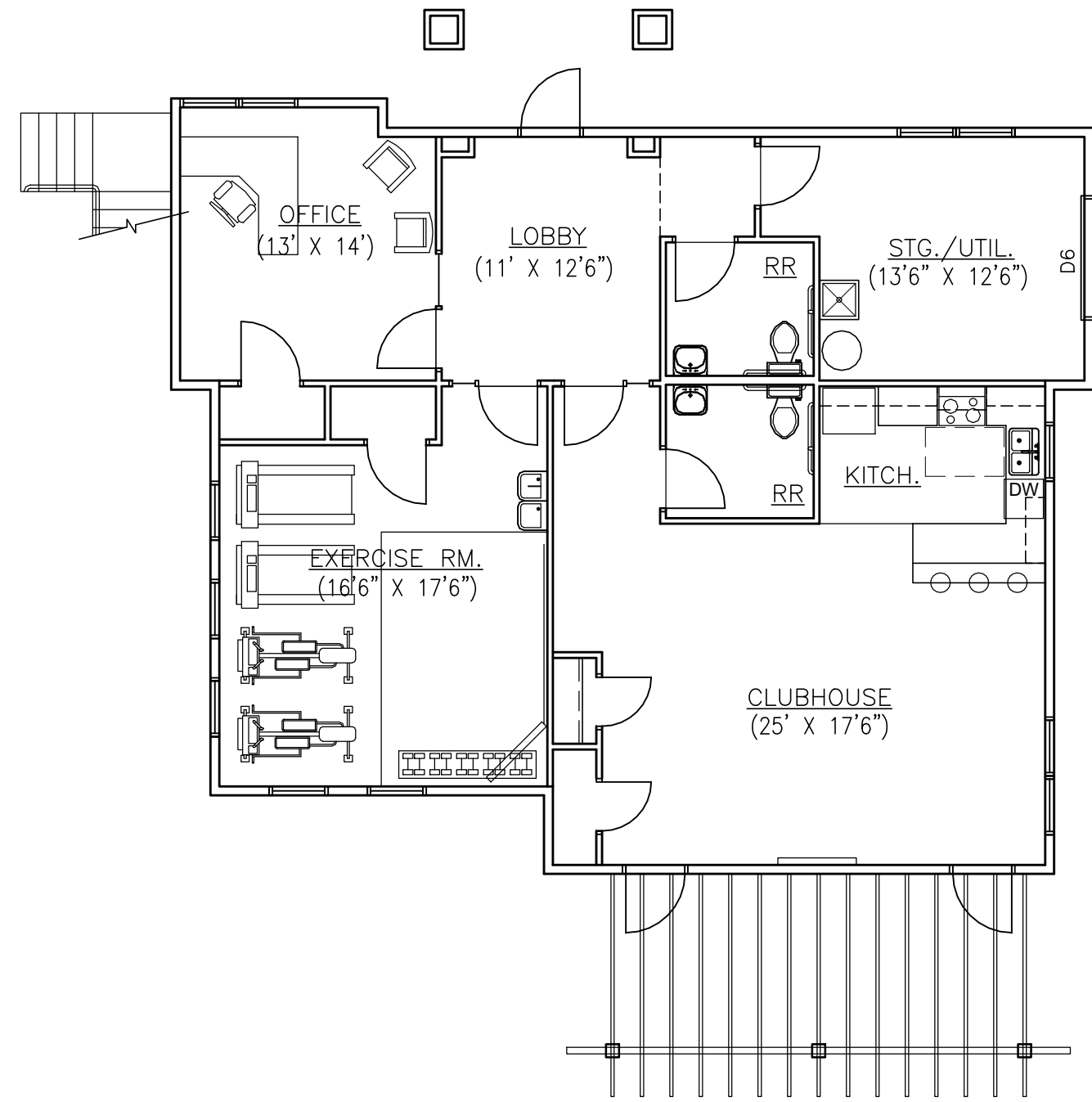
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FIRST / SECOND/ THIRD FLOOR PLAN - 24-PLEX
SCALE: 1/8" = 1'-0"



UPPER FLOOR PLAN
SCALE: 1/8" = 1'-0"



LOWER FLOOR PLAN
SCALE: 1/8" = 1'-0"

CLUBHOUSE FLOOR PLAN
SCALE: 1/8" = 1'-0"

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8/8a



8/8a



8/8a



8/8a



8/8a



8/8a



8/8a



8/8a



8/8a



8/8a



8/8a



8/8a



8/8a



8/8a



8/8a





Project/File: **Station Village Apartments / BOI16-0418 / PUD16-00027**
The applicant has submitted a PUD application to the City of Boise for 91 apartment units.

Lead Agency: City of Boise

Site address: 2350 W. Kootenai Street

Staff Approval: October 17, 2016

Applicant: Donna Jacobs
Vista Village, LLC
PO Box 8286
Boise, ID 83707

Representative: Robert Powell
Dave Evans Construction
7761 W. Riverside Drive, #100
Boise, ID 83714

Staff Contact: Christy Little
Phone: 387-6144
E-mail: clittle@achdidaho.org

A. Findings of Fact

- Description of Application:** The applicant is proposing to construct 91 apartment units on 7.39 acres. The site is zoned R-1C.

- Description of Adjacent Surrounding Area:**

Direction	Land Use	Zoning
North	Single family residential	R-1C
South	Single family residential	R-1C
East	Railroad	
West	Commercial – Vista Village, Rite Aid, Jack-in-the-Box, etc..	C-2D

- Site History:** ACHD has not previously reviewed this site for a development application. .
- Transit:** Transit services are available to serve this site. Route 3 runs on Vista Avenue, with bus stops located at Vista/Kootenai and Vista/Cassia. Route 29 on Federal Way serves the BSU campus, with a bus stop located at Federal Way/Kootenai/Protest.
- New Center Lane Miles:** The proposed development includes no new centerline miles of new public road.

6. **Impact Fees:** There will be an impact fee that is assessed and due prior to issuance of any building permits. The assessed impact fee will be based on the impact fee ordinance that is in effect at that time.
7. **Capital Improvements Plan (CIP)/ Integrated Five Year Work Plan (IFYWP):**
- Kootenai Street is scheduled for construction in the IFYWP to be improved in 2017, with curb, gutter, sidewalks and bike lanes, on both sides of the road, from Vista Avenue to Federal Way. Existing on-street parking will be removed to accommodate the bike lanes.

B. Traffic Findings for Consideration

1. **Trip Generation:** This development is estimated to generate 605 vehicle trips per day (6.65 trips per unit); 56 vehicle trips per hour in the PM peak hour (0.62 trips per unit), based on the Institute of Transportation Engineers Trip Generation Manual, 9th Edition.
2. **Condition of Area Roadways**
Traffic Count is based on Vehicles per hour (VPH)

Roadway	Frontage	Functional Classification	PM Peak Hour Traffic Count	PM Peak Hour Level of Service
Vista Avenue	0-feet	Principal Arterial	1,001	Better than "E"
Kootenai Street (e/o Vista)	0-feet	Minor Arterial (2-3 lanes)	562	Better than "E"
Kootenai Street (w/o Federal Way)	20-feet	Minor Arterial	663	"E"
Robert Street	310-feet	Local	N/A	N/A
Day Drive	110-feet	Local	N/A	N/A
Cassia Street	0-feet	Local	N/A	N/A

* Acceptable level of service for a five-lane principal arterial is "E" (1,780 VPH).

* Acceptable level of service for a two-lane minor arterial is "E" (575 VPH).

* Acceptable level of service for a three-lane minor arterial is "E" (720 VPH)

3. **Average Daily Traffic Count (VDT)**

Average daily traffic counts are based on ACHD's most current traffic counts.

- The average daily traffic count for Robert Street north of Kootenai Street was 1,180 on August 24, 2016. The noticeable peak time on this street is from 12-1PM, which correlates to the fast food uses on the west side of Robert Street.
- The average daily traffic count for Day Drive east of Vista Avenue was 708 on August 24, 2016.
- The average daily traffic count for Cassia Street east of Vista Avenue was 542 on August 24, 2016.
- The average daily traffic count for Robert Street south of Cassia Street was 452 on August 24, 2016.
- These traffic count locations are shown on the map below, as indicated by the red circle.



- The average daily traffic count for Kootenai Street east of Vista Avenue was 10,355 on February 18, 2016.
- The average daily traffic count for Kootenai Street west of Federal Way was 11,536 on February 18, 2016.
- The average daily traffic count for Vista Avenue south of Rosehill Street was 20,016 on February 28, 2015.

C. Findings for Consideration

1. Kootenai Street

- Existing Conditions:** Kootenai Street is improved with 2-travel lanes, and vertical curb and gutter. There are short segments of sidewalk. There is on-street parking.
- Policy:**

Arterial Roadway Policy: District Policy 7205.2.1 states that the developer is responsible for improving all street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets.

Master Street Map and Typology Policy: District Policy 7205.5 states that the design of improvements for arterials shall be in accordance with District standards, including the Master Street Map and Livable Streets Design Guide. The developer or engineer should contact the District before starting any design.

ACHD Master Street Map: ACHD Policy Section 3111.1 requires the Master Street Map (MSM) guide the right-of-way acquisition, arterial street requirements, and specific roadway features required through development. This segment of Kootenai Street is designated in the MSM as a Residential Arterial with 2-lanes and no planned widening.

Sidewalk Policy: District Policy 7205.5.7 requires a concrete sidewalk at least 5-feet wide to be constructed on both sides of all arterial streets. A parkway strip at least 6-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District's planter width policy if trees are to be placed within the parkway strip. Sidewalks constructed next to the back-of-curb shall be a minimum of 7-feet wide.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

Minor Improvements Policy: District Policy 7203.3 states that minor improvements to existing streets adjacent to a proposed development may be required. These improvements are to correct deficiencies or replace deteriorated facilities. Included are sidewalk construction or replacement; curb and gutter construction or replacement; replacement of unused driveways with curb, gutter and sidewalk; installation or reconstruction of pedestrian ramps; pavement repairs; signs; traffic control devices; and other similar items.

- c. **Staff Comments/Recommendations:** The applicant has only 20-feet of frontage on Kootenai Street. ACHD will be improving Kootenai Street in 2017 with curb, gutter and sidewalk. In lieu of constructing the required sidewalk, the applicant should provide ACHD with a road trust deposit in the amount of \$500. ACHD will construct the sidewalk with the project.

2. Robert Street

- a. **Existing Conditions:** Robert Street is improved with a 29-foot street section with curb, gutter and 5-foot wide concrete sidewalk from Day Drive, to the south for 200-feet. For the remainder of the frontage (110-feet), there is no curb, gutter or sidewalk.

- b. **Policy:**

Local Roadway Policy: District Policy 7207.2.1 states that the developer is responsible for improving all local street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets.

Street Section and Right-of-Way Policy: District Policy 7207.5 states that right-of-way widths for all local streets shall generally not be less than 50-feet wide and that the standard street section shall be 36-feet (back-of-curb to back-of-curb). The District will consider the utilization of a street width less than 36-feet with written fire department approval.

Standard Urban Local Street—36-foot to 33-foot Street Section and Right-of-way Policy: District Policy 7207.5.2 states that the standard street section shall be 36-feet (back-of-curb to back-of-curb) for developments with any buildable lot that is less than 1 acre in size. This street section shall include curb, gutter, and minimum 5-foot concrete sidewalks on both sides and shall typically be within 50-feet of right-of-way.

The District will also consider the utilization of a street width less than 36-feet with written fire department approval. Most often this width is a 33-foot street section (back-of-curb to back-of-curb) for developments with any buildable lot that is less than 1 acre in size.

Sidewalk Policy: District Policy 7207.5.7 states that five-foot wide concrete sidewalk is required on both sides of all local street, except those in rural developments with net densities

of one dwelling unit per 1.0 acre or less, or in hillside conditions where there is no direct lot frontage, in which case a sidewalk shall be constructed along one side of the street. Some local jurisdictions may require wider sidewalks.

The sidewalk may be placed next to the back-of-curb. Where feasible, a parkway strip at least 8-feet wide between the back-of-curb and the street edge of the sidewalk is recommended to provide increased safety and protection of pedestrians and to allow for the planting of trees in accordance with the District's Tree Planting Policy. If no trees are to be planted in the parkway strip, the applicant may submit a request to the District, with justification, to reduce the width of the parkway strip.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

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- c. **Staff Comments/Recommendations:** The applicant should improve Robert Street as ½ of a 29-foot street section with vertical curb, gutter and 5-foot wide concrete sidewalk to match the existing improvements. The applicant should dedicate right-of-way to 2-feet behind the back of sidewalk. Parking should be restricted on this segment of Robert Street, and the applicant should install "NO PARKING" signs adjacent to their entire Robert Street frontage (including segment that is already improved).

3. Driveways

Kootenai Street Driveway

a. Policy:

- b. **Access Points Policy:** District Policy 7205.4.1 states that all access points associated with development applications shall be determined in accordance with the policies in this section and Section 7202. Access points shall be reviewed only for a development application that is being considered by the lead land use agency. Approved access points may be relocated and/or restricted in the future if the land use intensifies, changes, or the property redevelops.
- c. **Access Policy:** District policy 7205.4.6 states that direct access to minor arterials is typically prohibited. If a property has frontage on more than one street, access shall be taken from the street having the lesser functional classification. If it is necessary to take access to the higher classified street due to a lack of frontage, the minimum allowable spacing shall be based on Table 1a under District policy 7205.4.6, unless a waiver for the access point has been approved by the District Commission.
- d. **Successive Driveways:** District policy 7205.4.6 Table 1a, requires driveways located on minor arterial roadways with a speed limit of 25 MPH to align or offset a minimum of 330-feet from any existing or proposed driveway.
- e. **Driveway Width Policy:** District policy 7205.4.8 restricts high-volume driveways (100 VTD or more) to a maximum width of 36-feet and low-volume driveways (less than 100 VTD) to a maximum width of 30-feet. Curb return type driveways with 30-foot radii will be

required for high-volume driveways with 100 VTD or more. Curb return type driveways with 15-foot radii will be required for low-volume driveways with less than 100 VTD.

- f. **Driveway Paving Policy:** Graveled driveways abutting public streets create maintenance problems due to gravel being tracked onto the roadway. In accordance with District policy, 7205.4.8, the applicant should be required to pave the driveway its full width and at least 30-feet into the site beyond the edge of pavement of the roadway and install pavement tapers in accordance with Table 2 under District Policy 7205.4.8.
- g. **Cross Access Easements/Shared Access Policy:** District Policy 7202.4.1 states that cross access utilizes a single vehicular connection that serves two or more adjoining lots or parcels so that the driver does not need to re-enter the public street system.
- h. **Applicant's Proposal:** The applicant is proposing to construct a 25-foot wide driveway on Kootenai Street. This is the full width of the site frontage on Kootenai Street.
- i. **Staff Comments/Recommendations:** The proposed driveway is located 30-feet east of the Columbus Street/Kootenai Street intersection. District policy requires that this driveway be aligned with Columbus Street, or offset Columbus Street and other driveways by 330-feet. The driveways are offset such that vehicles turning left from Kootenai on to Columbus will conflict with left turns into the site off of Kootenai. There is no center turn lane on Kootenai Street, and a turn lane is not planned with the ACHD project due to the railroad tracks being located adjacent to the proposed driveway.

In pre-application meetings, staff recommended that the applicant work with the adjacent property owner to obtain additional property to locate a driveway in alignment with Columbus Street. The driveway should be constructed as a 24 to 30-foot wide curb return.

If that option is not feasible, then the driveway will need to be restricted to right-in only or right-in/right-out; or be restricted just to allow for emergency vehicles. If the driveway is right-in only, the applicant will need to design the driveway to discourage other maneuvers. If the driveway is right-in/right-out, the applicant will need to design an on-site median to restrict the left turning movements. Medians cannot be located in the right-of-way, and the driveway may not be wide enough for this configuration. If the driveway is emergency access only, then the applicant should restrict the driveway with bollards or gates, as determined by the Boise Fire Department.

Robert Street Driveway

a. Policy:

Driveway Location Policy: District policy 7207.4.1 requires driveways located near intersections to be located a minimum of 75-feet (measured centerline-to-centerline) from the nearest street intersection.

Successive Driveways: District Policy 7207.4.1 states that successive driveways away from an intersection shall have no minimum spacing requirements for access points along a local street, but the District does encourage shared access points where appropriate.

Driveway Width Policy: District policy 7207.4.3 states that where vertical curbs are required, residential driveways shall be restricted to a maximum width of 20-feet and may be constructed as curb-cut type driveways.

Driveway Paving Policy: Graveled driveways abutting public streets create maintenance problems due to gravel being tracked onto the roadway. In accordance with District policy, 7207.4.3, the applicant should be required to pave the driveway its full width and at least 30-feet into the site beyond the edge of pavement of the roadway.

- b. **Applicant's Proposal:** The applicant is proposing to utilize the existing the existing driveway on Robert Street located 175-feet south of Day Drive. No new driveways are proposed.

- c. **Staff Comments/Recommendations:** The existing driveway on Robert Street is approved.

4. Local Streets

- a. **Function:** The primary function of a local street is to serve adjacent property. Adjacent property will usually have unrestricted access to the street and ADT will typically be less than 2,000. Access to local streets is generally unrestricted, except near intersections.
- b. **Average Daily Traffic (ADT):** ADT on new and existing local streets should typically be less than 2,000. This ADT applies to both existing and new streets. For new streets that are stubbed to connect to adjacent land that is not fully developed, the allowable ADT for the new street will typically be no more than 1,000 ADT, to accommodate future additional traffic from the adjacent land, depending on the location and type of the stub street and the location and type size of the adjacent undeveloped land. **When stub streets are connected and properties fully developed, local streets should not exceed 2,000 ADT. In developed areas where streets already exceed 2,000 ADT or are close to exceeding 2,000 ADT, the Commission may grant approval to exceed the 2,000 ADT based on existing zoning of undeveloped properties or infill development.** The Commission may also consider the need for additional roadway improvements or traffic calming to mitigate the additional traffic if necessary. The ADTs listed above are desirable planning thresholds for local streets, not roadway capacities. Actual roadway capacities are much higher than the planning thresholds.
- c. **Maximum Traffic on One Access:** If a proposed development only has one access to a public street that is a local street, or if it proposes to extend public streets from existing development with only one local street access to the public street system, the maximum forecast ADT to be allowed at any point on the local street access is 1,000 and is subject to fire department requirements for the provisions of a secondary access.
- d. **Staff Comments/Recommendations:** It is anticipated that traffic from this development will be dispersed multiple directions. Vehicles leaving the site can use Day Drive to Vista Avenue, and go northbound or southbound. Vehicles can also exit the site to Kootenai Street via Robert Street; or to Cassia Street via Robert Street. Total trip generation is estimated at 605 vehicles per day.

Many roadway segments in this area are considered Local/Commercial streets where there is not solely front-on housing. Robert Street south of Day Drive is commercial and does not have front-on housing. Day Drive west of Robert Street is commercial and does not have front-on housing. Cassia Street east of Vista Avenue is commercial and residential, with one house.

None of the local streets in the area will exceed the 2,000 vehicle trips per day (vpd) threshold that has been established for local streets.

Robert Street north of Kootenai Street - 1,180 vpd (This includes fast food trips.)

Robert Street south of Cassia Street – 452 vpd

Day Drive east of Vista Avenue - 708 vpd (This includes Vista Village trips.)

Cassia Street east of Vista Avenue - 542 vpd

5. Tree Planters

Tree Planter Policy: Tree Planter Policy: The District's Tree Planter Policy prohibits all trees in planters less than 8-feet in width without the installation of root barriers. Class II trees may be allowed in planters with a minimum width of 8-feet, and Class I and Class III trees may be allowed in planters with a minimum width of 10-feet.

6. Landscaping

Landscaping Policy: A license agreement is required for all landscaping proposed within ACHD right-of-way or easement areas. Trees shall be located no closer than 10-feet from all public storm drain facilities. Landscaping should be designed to eliminate site obstructions in the vision triangle at intersections. District Policy 5104.3.1 requires a 40-foot vision triangle and a 3-foot height restriction on all landscaping located at an uncontrolled intersection and a 50-foot offset from stop signs. Landscape plans are required with the submittal of civil plans and must meet all District requirements prior to signature of the final plat and/or approval of the civil plans.

D. Special Note to City of Boise

1. Parking

The applicant has proposed to provide 143 parking stalls to accommodate the residents and guests of the proposed apartment project. The City requires 119 stalls. The Institute of Transportation Engineers (ITE) Parking Generation Manual, 4th Edition, recommends 177 parking stalls for a 91 unit apartment project. The parking needs generated by this development should be provided on-site, as there is not adequate availability of on-street parking.

E. Site Specific Conditions of Approval

1. Provide a \$500 road trust deposit to ACHD for the sidewalk on Kootenai Street that will be constructed by ACHD in 2017.
2. Improve Robert Street as ½ of a 29-foot street section with vertical curb, gutter and 5-foot wide concrete sidewalk to match the existing improvements. Dedicate right-of-way to 2-feet behind the back of sidewalk.
3. Install “NO PARKING” signs on the entire Robert Street frontage.
4. Kootenai Street Driveway (choose one option)
 - a. To obtain a full access driveway the applicant should work with the adjacent property owner to locate a driveway in alignment with Columbus Street. The driveway should be constructed as a 24 to 30-foot wide curb return.
 - b. The driveway can be located on-site as proposed and operate as a right-in only driveway. The driveway will need to be designed to restrict all other turning movements and will need to be signed accordingly.
 - c. The driveway can be located on-site as proposed and operate as a right-in/right-out only driveway. The applicant will need to construct an on-site median to restrict left turning movements into and out of the site, and the driveway will need to be signed accordingly. Medians in the road or right-of-way cannot be constructed at this location to restrict left turning movements and therefore, there may not be adequate width to properly restrict the left turning movements, in which case this option is not feasible. Due to the narrow width of road, lane configurations, and turning movements in this area, if left turning movements cannot be fully restricted, then a right-in/right-out driveway cannot be constructed. This option is conditional upon a design that is approved by ACHD.
 - d. The driveway can be constructed as an emergency access only driveway. Restrict with gates or bollards as determined by the Boise Fire Department.
5. The existing driveway on Robert Street is approved.
6. Payment of impacts fees are due prior to issuance of a building permit.

7. Comply with all Standard Conditions of Approval.

F. Standard Conditions of Approval

1. All proposed irrigation facilities shall be located outside of the ACHD right-of-way (including all easements). Any existing irrigation facilities shall be relocated outside of the ACHD right-of-way (including all easements).
2. Private Utilities including sewer or water systems are prohibited from being located within the ACHD right-of-way.
3. In accordance with District policy, 7203.3, the applicant may be required to update any existing non-compliant pedestrian improvements abutting the site to meet current Americans with Disabilities Act (ADA) requirements. The applicant's engineer should provide documentation of ADA compliance to District Development Review staff for review.
4. Replace any existing damaged curb, gutter and sidewalk and any that may be damaged during the construction of the proposed development. Contact Construction Services at 387-6280 (with file number) for details.
5. A license agreement and compliance with the District's Tree Planter policy is required for all landscaping proposed within ACHD right-of-way or easement areas.
6. All utility relocation costs associated with improving street frontages abutting the site shall be borne by the developer.
7. It is the responsibility of the applicant to verify all existing utilities within the right-of-way. The applicant at no cost to ACHD shall repair existing utilities damaged by the applicant. The applicant shall be required to call DIGLINE (1-811-342-1585) at least two full business days prior to breaking ground within ACHD right-of-way. The applicant shall contact ACHD Traffic Operations 387-6190 in the event any ACHD conduits (spare or filled) are compromised during any phase of construction.
8. Utility street cuts in pavement less than five years old are not allowed unless approved in writing by the District. Contact the District's Utility Coordinator at 387-6258 (with file numbers) for details.
9. All design and construction shall be in accordance with the ACHD Policy Manual, ISPWC Standards and approved supplements, Construction Services procedures and all applicable ACHD Standards unless specifically waived herein. An engineer registered in the State of Idaho shall prepare and certify all improvement plans.
10. Construction, use and property development shall be in conformance with all applicable requirements of ACHD prior to District approval for occupancy.
11. No change in the terms and conditions of this approval shall be valid unless they are in writing and signed by the applicant or the applicant's authorized representative and an authorized representative of ACHD. The burden shall be upon the applicant to obtain written confirmation of any change from ACHD.
12. If the site plan or use should change in the future, ACHD Planning Review will review the site plan and may require additional improvements to the transportation system at that time. Any change in the planned use of the property which is the subject of this application, shall require the applicant to comply with ACHD Policy and Standard Conditions of Approval in place at that time unless a waiver/variance of the requirements or other legal relief is granted by the ACHD Commission.

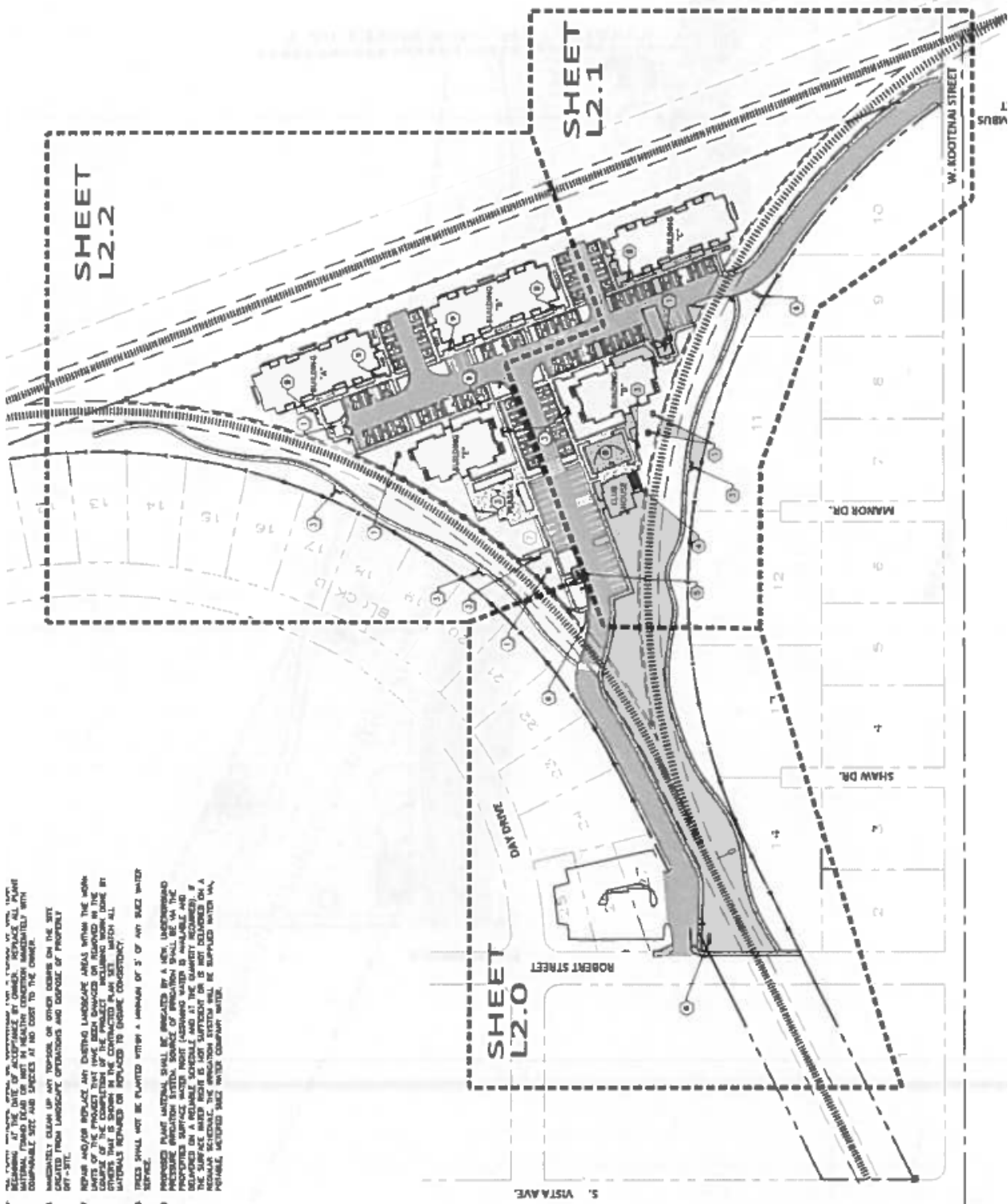
G. Conclusions of Law

1. The proposed site plan is approved, if all of the Site Specific and Standard Conditions of Approval are satisfied.
2. ACHD requirements are intended to assure that the proposed use/development will not place an undue burden on the existing vehicular transportation system within the vicinity impacted by the proposed development.

H. Attachments

1. Vicinity Map
2. Site Plan
3. Utility Coordinating Council
4. Development Process Checklist
5. Appeal Guidelines





Developer/Local Improvement District Right of Way Improvements Guideline Request

Purpose: To develop the necessary avenue for proper notification to utilities of local highway and road improvements, to help the utilities in budgeting and to clarify the already existing process.

- 1) **Notification:** Within five (5) working days upon notification of required right of way improvements by Highway entities, developers shall provide written notification to the affected utility owners and the Ada County Utility Coordinating Council (UCC). Notification shall include but not be limited to, project limits, scope of roadway improvements/project, anticipated construction dates, and any portions critical to the right of way improvements and coordination of utilities.
- 2) **Plan Review:** The developer shall provide the highway entities and all utility owners with preliminary project plans and schedule a plan review conference. Depending on the scale of utility improvements, a plan review conference may not be necessary, as determined by the utility owners. Conference notification shall also be sent to the UCC. During the review meeting the developer shall notify utilities of the status of right of way/easement acquisition necessary for their project. At the plan review conference each company shall have the right to appeal, adjust and/or negotiate with the developer on its own behalf. Each utility shall provide the developer with a letter of review indicating the costs and time required for relocation of its facilities. Said letter of review is to be provided within thirty calendar days after the date of the plan review conference.
- 3) **Revisions:** The developer is responsible to provide utilities with any revisions to preliminary plans. Utilities may request an updated plan review meeting if revisions are made in the preliminary plans which affect the utility relocation requirements. Utilities shall have thirty days after receiving the revisions to review and comment thereon.
- 4) **Final Notification:** The developer will provide highway entities, utility owners and the UCC with final notification of its intent to proceed with right of way improvements and include the anticipated date work will commence. This notification shall indicate that the work to be performed shall be pursuant to final approved plans by the highway entity. The developer shall schedule a preconstruction meeting prior to right of way improvements. Utility relocation activity shall be completed within the times established during the preconstruction meeting, unless otherwise agreed upon.

Notification to the Ada County UCC can be sent to: 50 S. Cole Rd. Boise 83707, or Visit iducc.com for e-mail notification information.

Development Process Checklist

Items Completed to Date:

- ☒ Submit a development application to a City or to Ada County
- ☒ The City or the County will transmit the development application to ACHD
- ☒ The ACHD **Planning Review Section** will receive the development application to review
- ☒ The **Planning Review Section** will do one of the following:
 - ☐ Send a “**No Review**” letter to the applicant stating that there are no site specific conditions of approval at this time.
 - ☒ Write a **Staff Level** report analyzing the impacts of the development on the transportation system and evaluating the proposal for its conformance to District Policy.
 - ☐ Write a **Commission Level** report analyzing the impacts of the development on the transportation system and evaluating the proposal for its conformance to District Policy.

Items to be completed by Applicant:

- ☐ For **ALL** development applications, including those receiving a “**No Review**” letter:
 - The applicant should submit one set of engineered plans directly to ACHD for review by the **Development Review Section** for plan review and assessment of impact fees. (Note: if there are no site improvements required by ACHD, then architectural plans may be submitted for purposes of impact fee assessment.)
 - The applicant is required to get a permit from Construction Services (ACHD) for ANY work in the right-of-way, including, but not limited to, driveway approaches, street improvements and utility cuts.
- ☐ Pay Impact Fees prior to issuance of building permit. Impact fees cannot be paid prior to plan review approval.

DID YOU REMEMBER:

Construction (Non-Subdivisions)

☐ **Driveway or Property Approach(s)**

- Submit a “Driveway Approach Request” form to ACHD Construction (for approval by Development Services & Traffic Services). There is a one week turnaround for this approval.

☐ **Working in the ACHD Right-of-Way**

- Four business days prior to starting work have a bonded contractor submit a “Temporary Highway Use Permit Application” to ACHD Construction – Permits along with:
 - a) Traffic Control Plan
 - b) An Erosion & Sediment Control Narrative & Plat, done by a Certified Plan Designer, if trench is >50' or you are placing >600 sf of concrete or asphalt.

Construction (Subdivisions)

☐ **Sediment & Erosion Submittal**

- At least one week prior to setting up a Pre-Construction Meeting an Erosion & Sediment Control Narrative & Plan, done by a Certified Plan Designer, must be turned into ACHD Construction to be reviewed and approved by the ACHD Stormwater Section.

☐ **Idaho Power Company**

- Vic Steelman at Idaho Power must have his IPCO approved set of subdivision utility plans prior to Pre-Con being scheduled.

☐ **Final Approval from Development Services is required** prior to scheduling a Pre-Con.

Request for Appeal of Staff Decision

1. **Appeal of Staff Decision:** The Commission shall hear and decide appeals by an applicant of the final decision made by the Development Services Manager when it is alleged that the Development Services Manager did not properly apply this section 7101.6, did not consider all of the relevant facts presented, made an error of fact or law, abused discretion or acted arbitrarily and capriciously in the interpretation or enforcement of the ACHD Policy Manual.
 - a. **Filing Fee:** The Commission may, from time to time, set reasonable fees to be charged the applicant for the processing of appeals, to cover administrative costs.
 - b. **Initiation:** An appeal is initiated by the filing of a written notice of appeal with the Secretary of Highway Systems, which must be filed within ten (10) working days from the date of the decision that is the subject of the appeal. The notice of appeal shall refer to the decision being appealed, identify the appellant by name, address and telephone number and state the grounds for the appeal. The grounds shall include a written summary of the provisions of the policy relevant to the appeal and/or the facts and law relied upon and shall include a written argument in support of the appeal. The Commission shall not consider a notice of appeal that does not comply with the provisions of this subsection.
 - c. **Time to Reply:** The Development Services Manager shall have ten (10) working days from the date of the filing of the notice of appeal to reply to the notice of the appeal, and may during such time meet with the appellant to discuss the matter, and may also consider and/or modify the decision that is being appealed. A copy of the reply and any modifications to the decision being appealed will be provided to the appellant prior to the Commission hearing on the appeal.
 - d. **Notice of Hearing:** Unless otherwise agreed to by the appellant, the hearing of the appeal will be noticed and scheduled on the Commission agenda at a regular meeting to be held within thirty (30) days following the delivery to the appellant of the Development Services Manager's reply to the notice of appeal. A copy of the decision being appealed, the notice of appeal and the reply shall be delivered to the Commission at least one (1) week prior to the hearing.
 - e. **Action by Commission:** Following the hearing, the Commission shall either affirm or reverse, in whole or part, or otherwise modify, amend or supplement the decision being appealed, as such action is adequately supported by the law and evidence presented at the hearing.



Independent School District of Boise City #1 **8/8a**

Boundaries, Transportation, and Traffic Safety
8169 W Victory Rd - Boise, ID 83709
(208) 854-4167 Fax (208) 854-4011

RESPONSE TO PLANNING & DEVELOPMENT SERVICES

DATE: October 10, 2016

TO: PDSTransmittals@cityofboise.org

FROM: Lanette Daw, Supervisor Traffic Safety and Transportation

RE: PUD16-00027 & CAR16-00030 – Station Village Apartments

At the present time, the Developer and/or Owner have made arrangements to comply with all requirements of the Boise School District.

The schools currently assigned to the proposed project area are:

Elementary School: **Monroe**
Junior High School: **South**
High School: **Borah**

Comments Regarding Traffic Impact: **None**

Comments Regarding Safe Routes to School Impact: **None**

If you have any further questions, please feel free to contact this office.



Dennis Doan
Chief

City Hall West
333 N. Mark Stall Place
Boise, Idaho 83704-0644

Phone
208/570-6500

Fax
208/570-6586

TDD/TTY
800/377-3529

Web
www.cityofboise.org/fire



Mayor
David H. Bieter

City Council
President
Maryanne Jordan

Council Pro Tem
David Eberle

Elaine Clegg
Lauren McLean
TJ Thomson
Ben Quintana

8/8a

Fire Department

October 10, 2016

Leon Letson
PDS – Current Planning

Re: PUD16-00027; 2350 W. Kootenai St.

Dear Leon,

This is a Planned Unit Development request for a 91 - unit multi-family development.

The Boise Fire Department has reviewed and can approve the application subject to compliance with all of the following code requirements and conditions of approval. Any deviation from this plan is subject to Fire Department approval. Please note that unless stated otherwise, this memo represents the requirements of the International Fire Code (IFC) as adopted and amended by Ordinance 6308.

Comments:

1. Fire hydrants, capable of producing the required fire flow, shall be located so that no part of the structure is more than 600-feet from the hydrant. (IFC 507.3, IFC B105.2, IFC C105). Additional fire hydrants are required.
2. Signs shall be placed at all points of entry to the subdivision stating "Notice All Roads Are Fire Lanes Park Only in Designated Parking Areas". (IFC D103.6)
3. For streets having a width less than 29 feet back of curb to back of curb parking shall be restricted on both sides. A note on the face of the final plat is required noting the parking restriction prior to signing of the final plat by the Boise City Engineer. In addition, No Parking signs shall be installed in accordance with the requirements of the IFC. (BCC 7-01-32, IFC 503.8)
4. The proposed access roads are 25 feet wide. Where a fire hydrant is located on a fire apparatus access road, the minimum road width shall be 26 feet, exclusive of shoulders (IFC D103.1)

General Requirement:

Fire Department required fire hydrants, access, and street identification shall be installed prior to construction or storage of combustible materials on site. Provisions may be made for temporary access and identification measures.

Specific building construction requirements of the International Building Code, International Fire Code and Boise City Code will apply. However, these provisions are best addressed by a licensed Architect at time of building permit application.

Regards,

Ron L. Johnson
Division Chief – Assistant Fire Marshal
Boise Fire Department

UNION PACIFIC RAILROAD
1400 Douglas Street, Stop 1580
Omaha, Nebraska 68179

P 402 544 5761
F 402 997 3603
prmcgill@up.com

Patrick R. McGill/UPC Senior Counsel-Real Estate, Law Dept.

8/8a

November 7, 2016

VIA EMAIL ONLY: lletson@cityofboise.org

City of Boise
Attn: Leon Letson
150 N. Capitol Blvd.
Boise, Idaho 83701

Re: Comments to application for rezoning and construction of multi-family development at 2350 W. Kootenai St. & 1110 S. Robert Street, Applicant: Donna Jacobs (the "Project")

Dear Mr. Letson:

Thank you for allowing Union Pacific Railroad Company ("UP") the opportunity to submit the following comments in response to the notice on the above-referenced Project. UP is a Delaware corporation that owns and operates a common carrier railroad network in the western half of the United States, including the State of Idaho. UP's rail network is vital to the economic health of Idaho and the nation as a whole and rail service to customers in the Boise Area is crucial to the future success and growth of those customers.

The proposed Project location is adjacent to UP's Boise Subdivision at the center of a wye (triangular shaped arrangement of rail tracks). UP's lessee, Boise Valley Railroad, is the short line railroad which currently conducts freight rail operation on the tracks around the Project location. Any land planning decisions should consider that train volumes near the Project area may increase in the future. UP also asks that the City and the Project developers keep in mind that this is a vital rail corridor and nearby land uses should be compatible with this continuing rail use.

At-Grade Rail Crossing Safety

The safety of UP's employees, customers, adjoining land owners, and the communities we operate through is our top priority. At-grade rail crossings are areas where railroad operations and the public come into close contact. The proposed Project anticipates two at-grade crossings. UP objects to approval of this Project unless the planned at-grade vehicle crossings are put in place under terms acceptable to UP, including terms establishing the crossings as public crossings. Please note that no agreements between the applicant and UP have yet been reached, and Boise Valley Railroad is not authorized to enter into crossing agreements over the railroad wye or main line. We recommend that the City and developer notify the Idaho Public Utilities Commission (PUC) to schedule a diagnostic meeting regarding the proposed Project. Appropriate modifications to the proposed at-grade crossings may need to be included as part of the Project. Should the Project be approved, UP suggests the City consider holding railroad and crossing safety presentations, such as Operation Lifesaver, for the public on an appropriate basis.

Increased Traffic Impact

Rail crossing safety is critical to the public and to UP. Any increase in traffic from the Project may render inadequate the current safety devices in place on any nearby at-grade crossings. Additionally, an increase of pedestrian and vehicular traffic may conflict with train operations causing trains to proceed more slowly through the City, and/or make more frequent emergency stops, which would make rail service less effective and efficient. Should this Project be approved, UP requests that the developer and the City examine any increase in vehicular and pedestrian traffic and the impacts on any nearby at-grade road crossings to see if any additional mitigation measures should be included in the Project.

Trespassing

Any increase in pedestrian traffic will increase the likelihood of trespassing onto the railroad right-of-way. UP requests that the developer and the City examine the Project impacts associated with the increased likelihood of trespassing and set forth appropriate mitigation measures. In particular, UP requests that the developer install a sound wall along the main line and fencing around the legs of the wye to prevent individuals from trespassing onto the railroad tracks. Buffers and setbacks should also be considered adjacent to the right-of-way.

Noise and Vibration Impact

A railroad's 24-hour rail operations generate the noise and vibration one would expect from an active railway. Any increase in pedestrian and vehicular traffic over and around at-grade crossings may result in additional horn use by railroad employees. As a mitigation measure, the developer should disclose to the general public, including residents of the proposed development, the daytime and nighttime noise levels naturally occurring with rail service, including sounding horns at vehicle crossings where required, as well as the pre-existing and predictably-occurring vibration. These disclosures should note that train volume may increase in

the future. The Project's development plans should also include appropriate mitigation measures, such as construction of sound barrier walls or landscape buffers, and/or use of sound-proofing materials and techniques.

Drainage and Project Construction

UP requests the City ensure that the drainage plan relating to the Project does not shift storm water drainage toward UP property and infrastructure. Any runoff onto UP's property may cause damage to its facilities resulting in a potential public safety issue. If the Project is approved, we ask that the City require the applicant to mitigate all safety risks and the impacts of the railroad's 24-hour operations during the construction of the Project, including contacting Boise Valley Railroad to arrange for flaggers for work performed within twenty-five feet (25') of the nearest track.

UP appreciates the developer and the City giving due consideration to the above concerns, as this proposed Project may result in impacts to land use and public safety. Please give notice to UP of all future hearings and other matters with respect to the Project as follows:

Paul Nahas, Manager - Real Estate
Union Pacific Railroad Company
1400 Douglas Street - STOP 1690 Omaha, NE 68179
(402) 544-8043
plnahas@up.com

Please do not hesitate to contact Paul Nahas if you have any questions or concerns.

Sincerely,



Patrick R. McGill
Senior Counsel – Real Estate
Union Pacific Railroad Company

cc: Paul Nahas

Memo

To: Planning and Development Services
From: Evan Carpenter
Environmental Analyst
Public Works Department
Date: 9/30/16
Re: Solid Waste Comments – PUD16-00027

City of Boise Solid Waste staff has reviewed the application for this project and has the following comments:

The southern solid waste enclosure is ok as drawn.

The northern enclosure needs to have a shorter distance for the collection vehicle to travel in reverse (after servicing the dumpster, the vehicle must back up to an area where it can turn around). The current distance is unsafe as it is.

Please contact me with any questions at 388-4712.

CITY OF BOISE

INTER-DEPARTMENT
CORRESPONDENCE

Date: September 29, 2016

To: Planning and Development Services

From: Mike Sheppard, Civil Engineer
Public Works

Subject: PUD16-00027; 2350 W. Kootenai Street; Sewer Comments

Connection to central sewer is required. Sanitary sewers are available in W. Kootenai Street.

Prior to granting of final sewer construction plan approval, all requirements by Boise City Planning and Development Services must be met.

If you have any further questions, please contact Mike Sheppard at 384-3920.

CITY OF BOISE

INTER-DEPARTMENT CORRESPONDENCE

Date: 29 September 2016

To: Planning and Development Services

From: Tom Marshall, Street Light Technician
Public Works

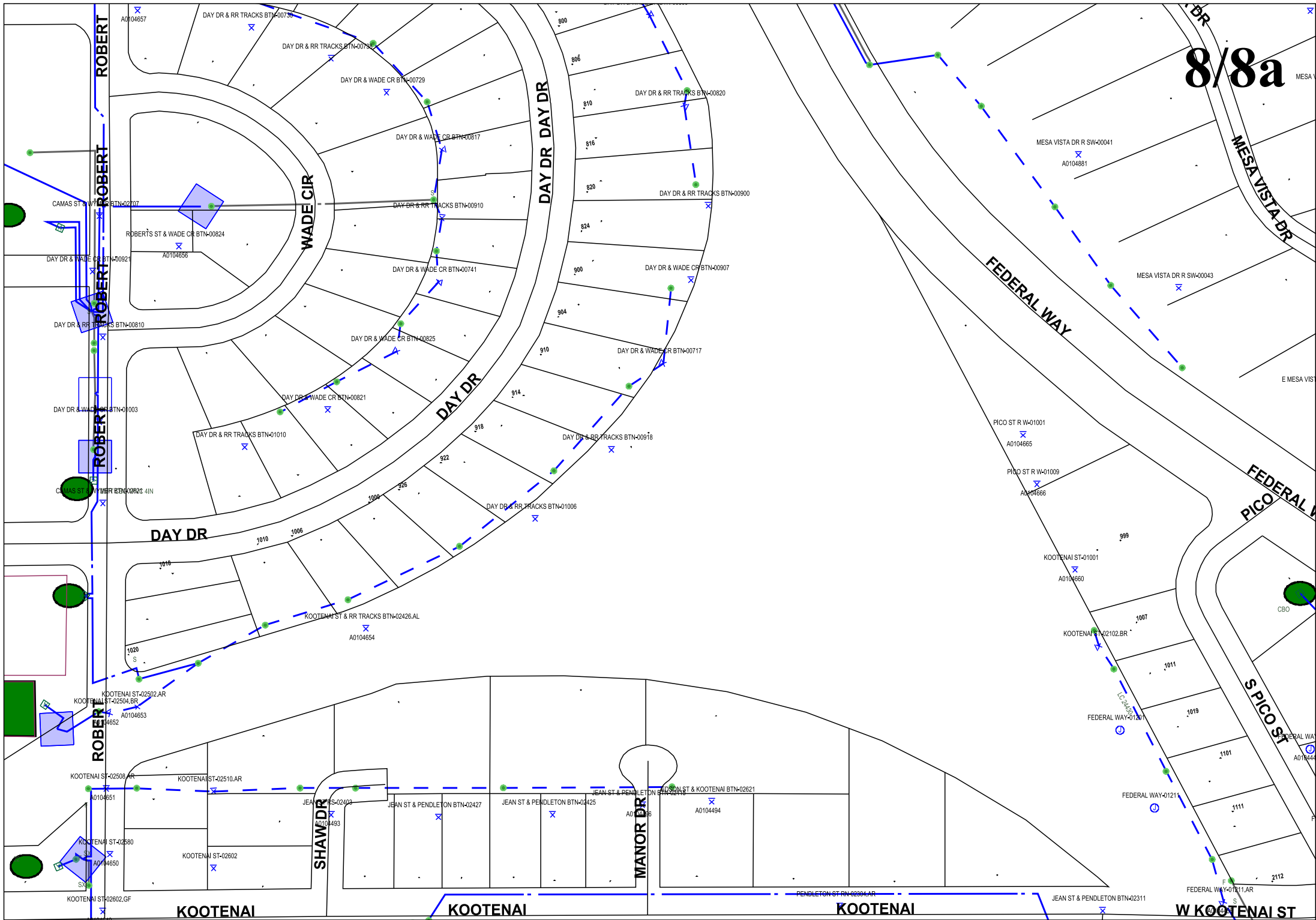
Subject: PUD16-00027; 2350 W Kootenai St; Street Light Comments

Street lights are required. Plans for this private street light system must be submitted, reviewed, and approved by Public Works prior to issuance of a building permit. A Homeowners' Association shall be created to be responsible for the ongoing operation and maintenance of the system.

New Street Light installations shall conform to the 2015 version of the Boise Standard Revisions, Idaho Standards for Public Works Construction (ISPMC) using approved LED fixtures listed in Attachment A to the Boise Standard Revisions.

Developer shall not connect, or allow any subcontractor to connect any irrigation timers, decorative lighting, entrance lighting, outlets or other electrical devices to any street lighting circuits. Any and all irrigation timers, decorative lighting, entrance lighting, outlets or other electrical devices shall be connected directly to Idaho Power at an Idaho Power approved location.

If you have any further questions contact Tom Marshall at 388-4719 or tmarshall@cityofboise.org.



CITY OF BOISE

INTER-DEPARTMENT CORRESPONDENCE

Date:9/29/2016

To: Planning and Development Services

From: Brian Murphy, Drainage Coordinator
Public Works

Subject: **PUD16-00027**; Drainage/Stormwater Comments

A drainage plan must be submitted and approved by Public Works prior to issuance of a building permit.

If you have any further questions contact Brian Murphy, 384-3752.

RYAN P. ARMBRUSTER

251 East Front Street, Suite 300
Post Office Box 1539
Boise, Idaho 83701
Telephone 208 343-5454
Fax 208 384-5844
E-mail rpa@elamburke.com

July 12, 2016

BOISE CITY PLANNING &
DEVELOPMENT DEPARTMENT
150 North Capitol Boulevard
Boise, Idaho 83701-0500

RE: PUD16-00027
Donna Jacobs
2350 W. Kootenai Street

Ladies and Gentlemen:

The above-referenced application to construct a 91 unit, multi-family development at the above-referenced address has been received in this office. This law firm represents the interests of Ada County Drainage District No. 3 (the "District"). The project site lies within the District's boundaries.

Providing all drainage will continue to be retained onsite, the District has no comment on the project located at 2350 W. Kootenai Street.

The District is responsible for ensuring that its system complies with conditions of a National Pollution Discharge Elimination System ("NPDES") permit issued by the Environmental Protection Agency to the District and other co-permittees, with regard to the quality of storm water runoff.

Approval of any proposed development is based upon the following conditions. Any proposed development must meet the storm water requirements of the Ada County Highway District ("ACHD") (if proposal is for a residential subdivision), or Boise City (if the proposal is for commercial, industrial, multi-family housing, or residential with private streets). This includes any and all requirements pertaining to on-site water detention, water quality treatment, and operation and maintenance. The project may also require a permit from the United States Army Corps of Engineers under their Section 404 permit program. If the work requires a permit from the Corps, the applicant will need to obtain their approval before starting work.

These requirements are outlined in the ACHD Policy Manual and the Boise City Storm Water Management and Discharge Control Ordinance, the Boise City Storm Water Design Standards Manual, and the Boise City Operation and Maintenance Guidance document.

The objectives of these requirements are to adequately control the quantity and quality of storm water runoff into the District's system and public waters. Compliance with these requirements will also address discharge limitations of "no net increase" in sediment and bacteria, required by the Lower Boise River Total Maximum Daily Load and the Idaho Department of Environmental Quality's "no net increase" policy.

Additionally, the District must be notified of any conditions that result in a significant change to the quantity or quality of the storm water runoff from this site.

If you have any questions or comment concerning the above, please feel free to contact me. Thank you for your assistance.

Very truly yours,

ELAM & BURKE
A Professional Association

*Sent without signature
to avoid delay*

Ryan P. Armbruster

RPA/ksk

c: District Commissioners
Steve Sweet
Dean Callen

CITY OF BOISE

INTER-DEPARTMENT CORRESPONDENCE

Date: 10/3/2016

To: Planning and Development Services

From: Terry Alber, Pretreatment Coordinator, 384-3992
Public Works

Subject: **PUD16-00027**; 2350 W KOOTENAI ST; Pretreatment Comments

Public Works, Pretreatment offers NO COMMENT.

From: Sal Sherman <slytrbl@gmail.com>
Sent: Tuesday, November 01, 2016 12:05 PM
To: Leon Letson
Subject: Village Station

I've lived on S. Day Drive for 25 years. The infrastructure will NOT support the additional traffic expected on S. Day or Robert St. A lovely, quiet, neighborhood will be ruined.

Sally Sherman

From: Marc Morin <marcnmotion@gmail.com>
Sent: Thursday, November 03, 2016 2:57 PM
To: Leon Letson
Subject: CAR16-00030 & PUD16-00027

Dear Mr. Letson:

I am writing regarding my concerns about the planning, and possible approval of the construction of the 91-unit apartments mentioned above. My concerns are that the existing infrastructure of this area is not capable of facilitating the influx of traffic that the addition of over 100 motor vehicles (in the vicinity and neighborhood) would create.

I live on Day Dr. If the mentioned proposal were to be approved, I hope that steps to mitigate traffic (such as speed bumps) on Day Dr. would be considered, and taken into account.

I hope these topics will be discussed at the public hearing on November 7th.

Sincerely, Marc Morin
810 S Day Dr
Boise, ID 83705
marcnmtion@gmail.com
[\(406\) 544 3011](tel:(406)5443011)

From: Amy Gardner <amyegardner72@gmail.com>
Sent: Tuesday, November 22, 2016 6:56 PM
To: Leon Letson
Subject: RE: case# CAR16-00027

Hello- We are the owners of 1010 Day Dr. Boise, ID 83705. This home has been in our family since the 1940's. We love the quiet, quaint bench location with longtime residents as our neighbors. We even had the rare site of a wild turkey visiting our backyard on a few different occasions this summer while seeing a neat caboose parked on the Union Pacific Rail Road tracks out back.

We have been to the neighborhood meeting on the proposed apartments. We strongly oppose them being built for many reasons. Here are a few listed below.

- Proposed Road: this will run right along our property line out back (literally, almost right next to our back fence line)**
- Increase in traffic: We believe that Robert St. & Day Dr. will have a huge influx of car traffic as well as Kootenai St. This will directly impact our homes.**
- Lights: From the proposed road, parking lot, cars driving by & the apartments....these would shine brightly in to our yards & homes.**
- No fence/barrier or wall: They have NO plans of putting up any type of barrier to protect our homes from the proposed street or apartment complex (cars driving by at all hours of the day & night, foot traffic, etc. right at our back fence line).**
- View: We currently look out to a grassy meadow-common area with views of the foothills & Table Rock. Our view with the proposed apartment complex would be of a busy road, cars driving by, parking lot & apartments. Oh...and lots of people.**
- Taxes: It is very likely that we would see an increase in our property taxes**
- Privacy: 2 & 3 story apartment buildings would take away from our peace & serenity in our yards & homes (they could look down & see into our yards & the inside of our homes from their apartment)**

I hope you take all of our thoughts & concerns into consideration when making a decision such as this. Please put yourself in our shoes & imagine this being built in your own backyard.

Thank you,
Chris & Amy Gardner

From: Cathy Chant
Sent: November 21, 2016
To: Leon Letson
Subject: OPPOSE: Station Village Apartments / PUD16-00027

I have resided on S.Day Drive since 1987. I rented for 9yrs @ 907 Day; I rented for 11yrs @ 910 Day; and I rented for 2 1/2yrs @ 904 Day (Lot 16 Blk D/Day Vista Add) and purchased the house in 2010 and have lived there for 8yrs.

Many justified concerns were raised by those in attendance at the neighborhood meeting held on July 19, 2016. The following comments explain my opposition to this development project.

1) At the meeting we were informed apartment traffic direction would be “right only” entering & exiting at W.Kootenai St. Right only exit at Kootenai St pushes all apartment traffic towards S.Robert St and Vista Ave. Robert St is a narrow street without sidewalks for safe pedestrian traffic. Robert St is used for residential street parking. Robert St is used by large trucks to deliver, load & unload for the Vista Village Stores. Robert St has several drive-thru exits and parking lot exits from Vista Village. Robert St/Day Dr corner parking lot will be used for an additional apartment entry/exit (with proposed roadway directly behind 2 or 3 homes at their backyard property line) which will contribute to increased traffic on Robert St. There will also be drivers on Kootenai St who will learn to short-cut thru Robert St to bypass right turn at Vista.

If apartment development is approved, Kootenai exit should allow for “left turn” to allow a direct route to BSU (S.Protest Rd), Downtown (left @S.Federal Way) and Merchants/Business (right @S.Federal Way). (ACHD Projects- Kootenai St-2017 / Columbus St-2019)

2) Trails along the outside of Wye tracks (tracks are several feet above ground level at my property) adjacent to backyard property lines will invite more foot traffic. There was no ownership/responsibility with Day Development when asked about existing/future fencing at the backyards of properties along the tracks on Day Dr of which many still have the original 2 rows (waist height) of barbed wire. Who is responsible in the event of injury to public walking the trail area? We were informed the walking trails proposed would be inhabited with vegetation native to the area. I am concerned if the trail

8/8a
80x10
grounds will be maintained to ensure all intrusive tumbleweeds, grasses, weeds, sucker trees, etc., do not return. Every spring/summer I invest time & dollars to clear 80x10 feet of overgrowth behind my garage and back fence. The lot and land around the railroad track property has not been maintained regularly, even on a yearly basis and it is never 100% completed when it does occur. It has been a fire hazard for more years than not. Residential properties on corner of Day & Robert were an eyesore for many years until torn down for a parking lot. So I question property/land stewardship of Day Development.

3) Day Drive is a modest, quiet neighborhood without sidewalks.

The homes on the east side of S. Day Dr and the north side of W. Kootenai St back up to the Wye and have beautiful unobstructed (priceless) views of: a magnificent sunrise; purple mountains majesty during sunsets, snowcapped in winter; full rainbows; and the lighted cross at Table Rock. To see the freight trains rolling down the tracks daily is quite nostalgic. At my back fence we have positioned several old wooden ladders so that our 6 young grandsons can watch the trains as they roll by. Three story apartment buildings will completely block all views and the visual of the train from my home as well as all homes along the tracks south of my lot and the Kootenai homes adjacent to the Wye tracks. Three and two story apartment buildings will tower over every single property that is located adjacent to the railroad tracks. This is very intrusive to the privacy that we have all become accustomed to and greatly appreciate. I am concerned about home resale value due to the close proximity of the towering apartments.

4) The freight trains pass on the main railroad tracks several times a day. This lot of land at the Wye would be more suitable for a single level storage unit facility with access at Kootenai St.

Respectfully,

Cathy Chant